

Gas Worx Design Guidelines

Initial Submittal Copy



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Site Plan Integration

These Design Guidelines for the referenced mixed-use project are incorporated by reference as part of the PD/A site plan approved for this zoning district.

Site Plan Compliance

Substantial compliance with these Design Guidelines (which are adopted as part of the referenced PD/A District) is required. In the alternative, where alternative design concepts meeting the intent of these Guidelines are determined to be preferred instead of achieving substantial compliance, the Developer may request approval for such alternative from the Zoning Administrator or its designee. Such alternative shall be approved as an “Alternative Design Exception.”

Determination of substantial compliance (or approval of an Alternative Design Exception) shall occur at time of Incremental Site Plan approval. Such Incremental Site Plan submittal shall meet the requirements of Sec. 27-238(e)(3) (“Submission requirements for review by the zoning administrator”).

Disclaimer

Building footprints, parking layouts, internal circulation, and other information indicated on all development parcel thumbnail sketches, including renderings and design inspiration photography, are conceptual. Design details will be finalized at time of Incremental Site Plan approval. The depictions contained in these Design Guidelines are intended to generally illustrate parcel configuration only. Detailed parcel development parameters are regulated by the PD/A Site Plan and other applicable City of Tampa requirements. Parcel dimensions are estimated and approximate.



Project Vision

Gas Worx is an approximately 43-acre redevelopment at a strategic location in Tampa between the heart of historic Ybor City to the north and the Channel District to the south, where there has been significant growth in recent years. The area comprising the Gas Worx Project has seen little change for decades. It is dominated by older declining industrial uses, parking lots, the vacant 1960s era Tampa Park Apartments and former site of a Peoples Gas production facility. A bird's eye view of Tampa from the early twentieth century shows this site with a street grid typical of Ybor City with small urban blocks intersected by a north to south rail corridor and orange groves to the west. Some of this historic fabric remains; however, there have been significant disruptions to the historic grid of the city with the addition of major new streets and highways, together with heavy-handed Urban Renewal policies of the 1960s. The site of Gas Worx today feels like a large gap between Tampa's more vibrant neighborhoods to the north and south and the rapidly developing Encore community to the west.

The redevelopment of the Gas Worx district is an opportunity to reconnect key parts of the city by appropriately scaled infill development that transitions in height and scale from larger and taller buildings to the south of the Channel District boundary to the smaller scale, lower heights of buildings in historic Ybor City. With close proximity to the downtown urban core, plus existing and planned transit connections, the Comprehensive Plan envisions a compact city form of development for the area with high densities to promote housing and commercial uses. Combining the large land area and urban density in a transit-oriented planned development provides the opportunity to establish a fully amenitized, walkable neighborhood, suited to contemporary lifestyles, blended with the authenticity of historic Ybor City.

To support a sustainable community, several key elements are included in the master plan vision:

- o A diversity of building types, uses, scale and design.
- o Streets that are designed for walking and biking.
- o Provision for new transit connections.
- o New parks and community gathering spaces.



Gas Worx Development Block Plan



Proposed Bike and Trail Connections Plan



Gas Worx Conceptual Massing Diagrams



Southwest Perspective

Gas Worx Conceptual Massing Diagrams



Southeast Perspective

Gas Worx Conceptual Massing Diagrams



Northwest Perspective

Gas Worx Conceptual Massing Diagrams



Northeast Perspective

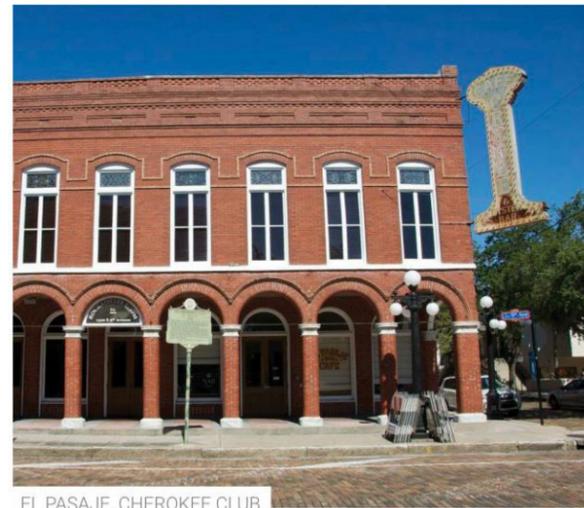
Architectural Character

The Historic District Parcels

The architecture in Gas Worx should be influenced by the design of buildings in historic Ybor City, however the building designs should also vary by location. The Gas Worx plan for redevelopment is in many ways about making transitions in the urban design pattern and fabric of the city. The blocks on the north side of the redevelopment area are within the Ybor City Historic District where the heights will step down to be compatible with the height of existing older buildings and the architectural expression will be inspired by the scale, patterns and materials of Ybor City. The architectural design should then be adjusted and transition in scale and design to the south where larger and mostly residential buildings start to merge with the more sizable buildings near the Channel District.

The design of buildings within the historic district will be influenced by both the large brick cigar factories and warehouses along with the smaller commercial and residential buildings in Ybor City. The larger existing buildings in Ybor City have an overall horizontal expression within which a repetition of vertical elements or windows are located. For example, a warehouse would typically have an overall brick surface with a consistent cornice line, however the windows are vertically proportioned and stacked on the façade to make vertical lines within the horizontal frame. Frequently, smaller structures often added later, rise above the consistent horizontal cornice line. This pattern and proportioning of elements along with varied rooflines should influence the design of new buildings in Gas Worx.

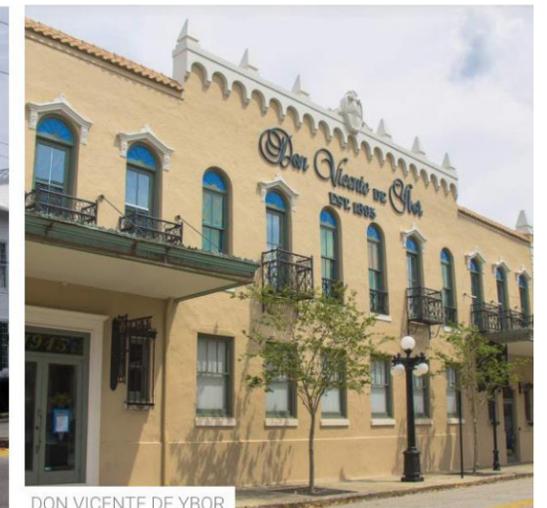
Where possible, new buildings will be aligned along sidewalks (existing easements may require setbacks at some locations) to make a distinctly urban condition and reinforce the grid that is being re-established in this part of the city. The buildings proposed for Gas Worx typically extend the full length of a block, however the design of the massing and façade treatments will articulate these block-long façades with varying heights and rooflines, subtle setbacks and changes of materials to break down the scale of the buildings. To be clear, the intent is to design a coherent and logical architectural expression that is varied, but is still one building and not a faux expression that resembles several buildings, an approach which is rarely successful. The buildings will be studied and designed taking into consideration eye level views along streets to test the interplay of shadowed setbacks, heights varying by one or two levels and a range of materials within a family of colors and textures. Another variation may be to extend the height of the first floor to create ground floor units that differ from the above floors, or to provide space to compliment and activate the street-level experience and to give the building a more prominent base visually.



EL PASAJE, CHEROKEE CLUB



HOME OF SWANN CIGARS



DON VICENTE DE YBOR



CHANCEY DESIGN BUILDING



HOTEL HAYA

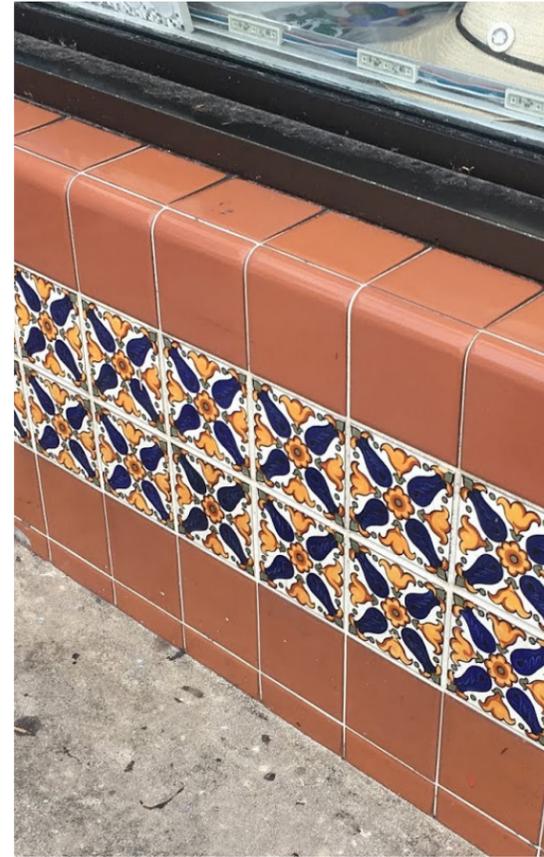
Architectural Character

Historic District Materials

The colors and materials of new buildings should be influenced by the texture of brick, stucco and other masonry surfaces in historic Ybor City as well as the darker and muted colors and tones as the predominant colors of Ybor City's older buildings. This palette of colors distinguishes Ybor City from newer parts of Tampa which typically have a lighter range of colors. At selected locations, such as building entries and in arcades, Ybor City has buildings with colorful ceramic tile and painted colors which should be considered for prominent locations at street level in proposed new buildings.

There are also character defining features of Ybor City buildings which can be adapted to these mostly residential buildings. From an urban design perspective there are key building locations, such as entries, and other prominent and highly visible locations, where new building designs should be varied with shaded overhangs, projections and canopies. These are useful in this climate and vary the design of buildings along the sidewalk while providing more pedestrian space. Metal balconies which are a light and lacy framework in front of solid masonry façades can be a valuable amenity for residential units and an appropriate way to recall the distinctive architecture of Ybor City. Signs and graphics on buildings using painted colors, illumination and large scale graphics (covered in another section of the Guidelines) are additional opportunities to make connections with the architecture of Ybor City.

Another opportunity to draw connections to the scale of Ybor City is to make small scale courtyards, even if private, which are visible from the public street. The openings to courtyards will make a break in the street wall to give variation along the sidewalk. Entries to these courtyards should be open air in this climate and be defined by metal gates, where required for security. These gates and metal fences should be setback from the sidewalk to give added public space along the sidewalk.



Architectural Character

Non-Historic/Transitional Parcels

In the area of Gas Worx south of the Ybor historic district the architectural character should be adjusted as development moves toward the elevated Selmon Expressway and the Channel District and downtown.

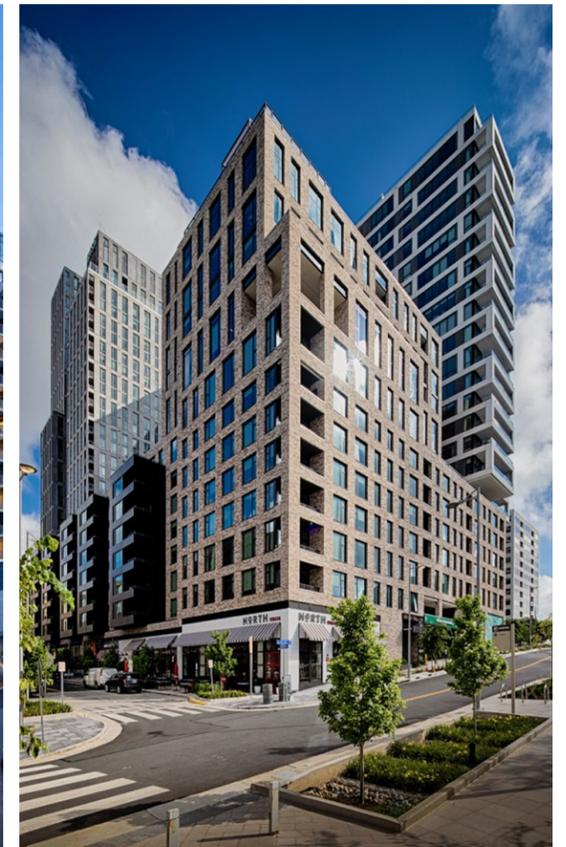
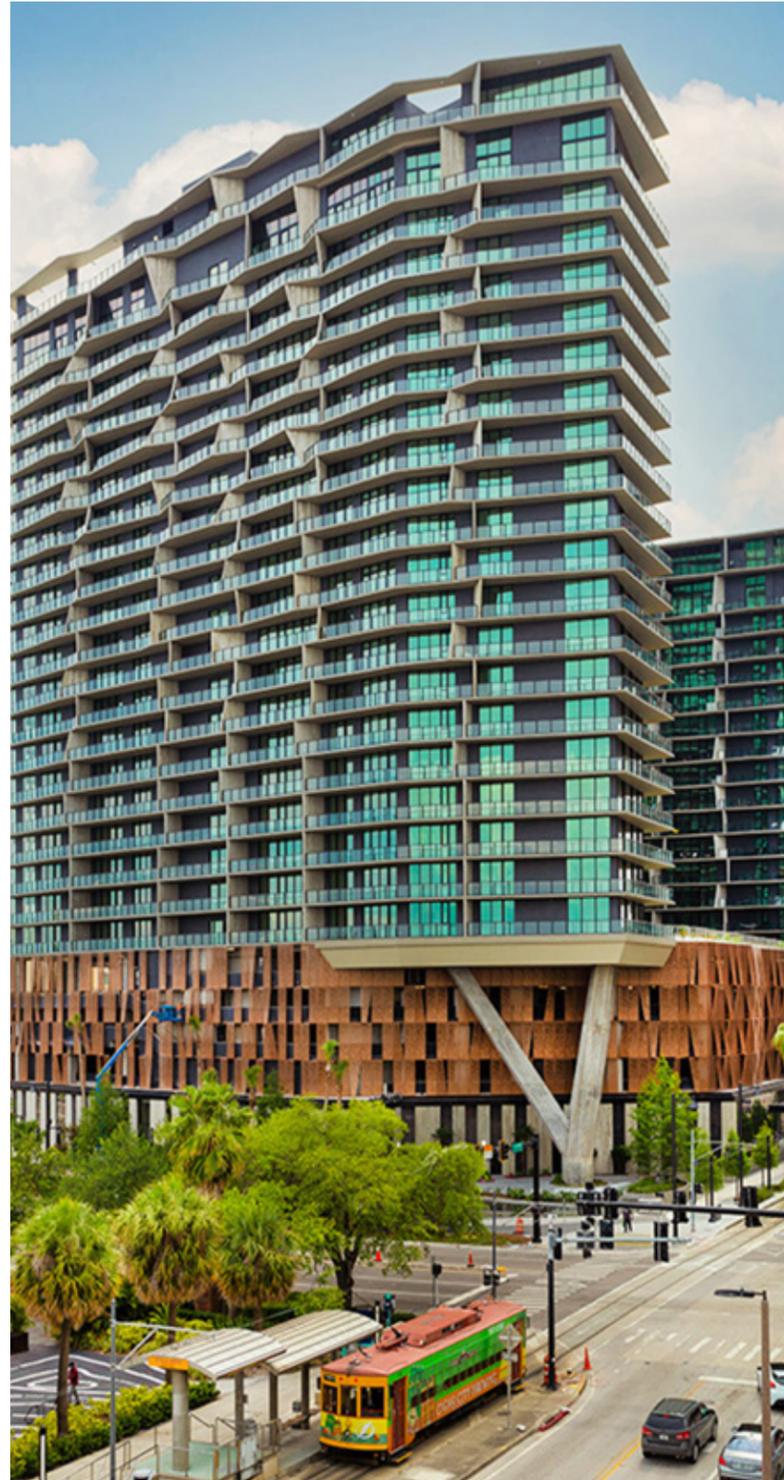
Parcels that abut the historic district will be used to transition the scale through use of mid-rise structures and/or using intentional step-ups in height within the block. Mid-rise development (typically five-to-ten stories) should be designed to be compatible with the historic buildings of Ybor but with restraint so as not to compete with, or artificially replicate authentically historic structures. New mixed-use, apartment and office buildings in this transitional zone have the opportunity to be designed to reflect a warehouse/industrial aesthetic, as a reference to the area's industrial past, and to distinguish it slightly yet relate to historic Ybor.



Architectural Character

Non-Historic/Transitional Parcels

Parcels that are along the southern edge of the district are impacted by the elevated Selmon Expressway and shipyard south of Adamo. There is also increased connectivity to Downtown, Encore and the Channel District via 12th Street and Nebraska Avenue. In this area a much larger scale and mass of building is appropriate. The architectural expression that is appropriate for mid-rise buildings typically requires a different approach for buildings that are taller. In these buildings there will likely be a base of five to six stories, which may recall some of the elements described above; however, it is expected that above that base, buildings will have a distinctly contemporary expression. Some subtle variation in the façades of taller buildings is a way to break down the scale of these larger buildings as well as an appropriate response to the varying solar exposure of buildings. For example, east and west facing façades may have limited glass or have sun screening integrated into the articulation, while northern facing façades may have the most glass to maximize access to natural light. The colors and materials of these taller buildings should be lighter in tone as a response to the climate and as a way to distinguish these buildings from the older and typically darker colors and muted tones of historic buildings in Ybor City.

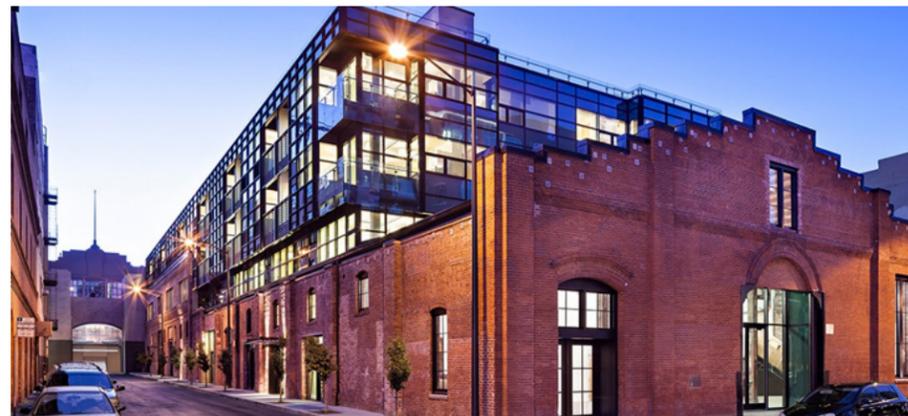


Architectural Character

Adaptive Reuse of Existing Structures

The plan area consists largely of single-story light industrial buildings, surface parking and empty lots. Most existing buildings were built in recent decades, are mostly functional in nature and do not represent Ybor's historic past. One exception is the large brick warehouse between 3rd and 4th Avenues and Channelside and 14th Street. While this building is not classified as an historic contributing structure, its rich red, brick double-barrel roof and interior clear spans lends itself to adaptive re-use in many ways. The building occupies the full block, approximately 70,000 square feet, and is large enough to accommodate a significant retail component, such as a food hall or market, while also having complementary uses, such as office, hospitality or residential.

The plan calls for the building to be partially repurposed but also have new, vertical structure creatively and sensitively added, so as to complement the original, patinaed brick façades. Moreover, subject to structural engineering validation, it is anticipated that portions of the warehouse can be opened up to create an accessible neighborhood amenity with seating and dining areas both inside and out. To complement this the 3rd Avenue pedestrian street to the south will be enhanced with shaded paved areas to support outdoor dining and entertainment.



Architectural Character

Retail and Groundplane Activation

The architectural integrity of Ybor City is also about the experience along the sidewalks. The pedestrian in Ybor City finds a rich sensory experience walking under canopies and balconies, with varied colors and materials, the smell of coffee, sound of people and music and authentic connection to the past. Clustering food and beverage tenants in a market-like setting at the heart of Gas Worx in a reused brick warehouse building aims to capture the spirit of Ybor City. The adjacent 3rd Avenue Paseo will allow the re-purposed warehouse to open up onto extensive outdoor dining spaces and public space for outdoor entertainment populated with tables and chairs and shaded by trees and canopies.

To complement this highly activated gathering center, additional retail should be placed on surrounding streets and at strategic corners. Within the blocks further south retail outlets within residential buildings will likely comprise of more neighborhood service outlets, such as grocery, drug store, dry cleaner and personal services. These residential buildings will, by design, also contribute to the experience of the streetscape by locating entries, common areas, recreational spaces and other amenity spaces along the sidewalk with transparency to position this activity where visible from the public realm. The aim is to expand and extend the feel and character of Ybor City in this new district while not copying or compromising the integrity of the authentic past. Moreover, as live-work lifestyle trends continue to evolve, retail spaces should have the flexibility to be utilized for non-conventional commercial purposes, such as co-working, online delivery pick-up, maker space or event space.

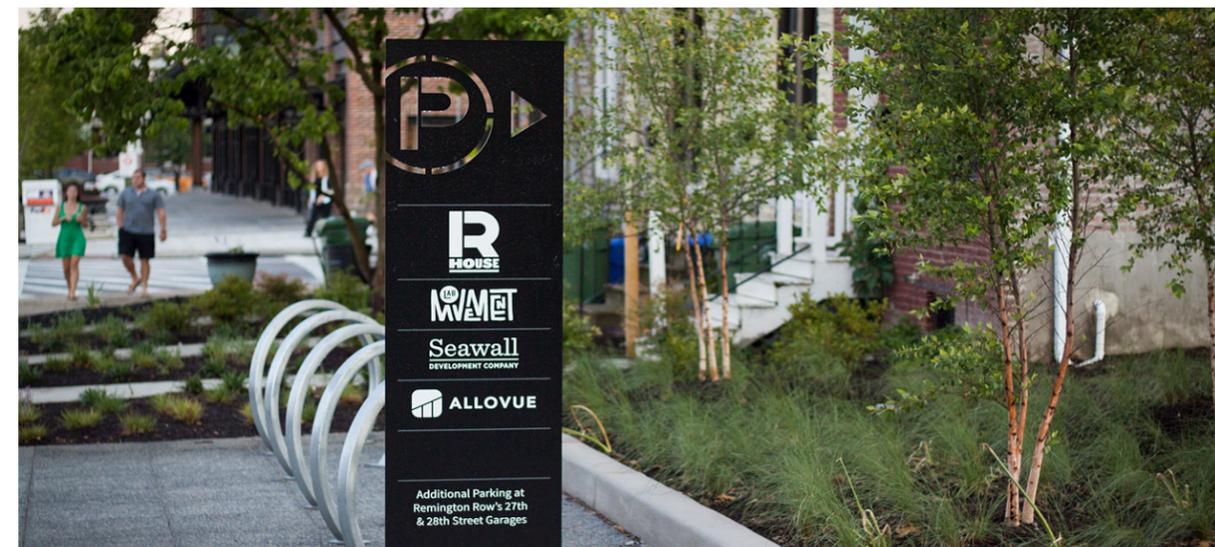
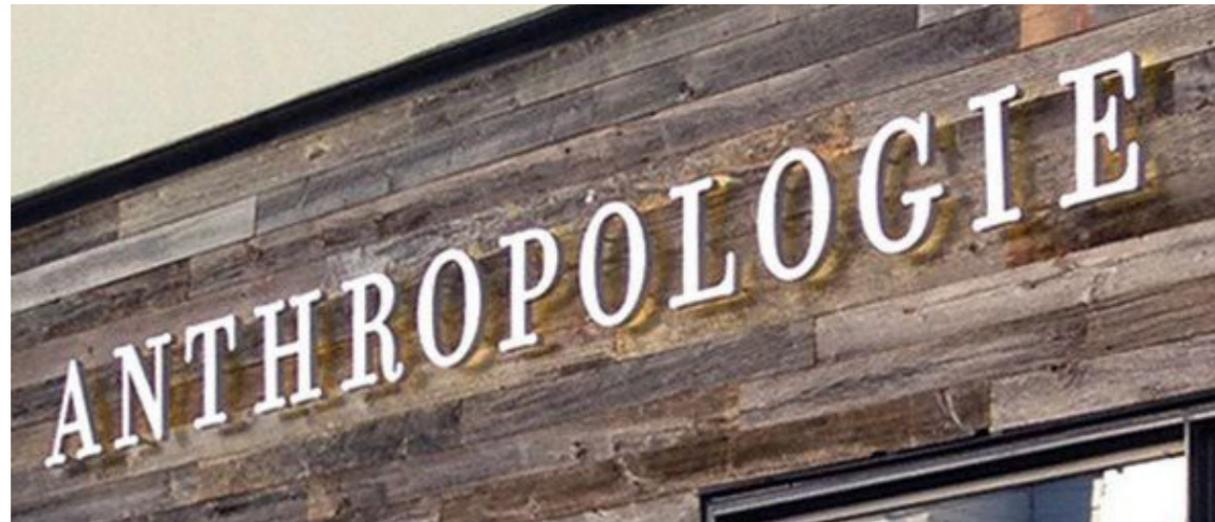


Signage

The signage throughout Gas Worx will integrate into the architecture to form a cohesive vision. The size, proportions, color, type and materiality of the signage will be carefully considered within the context and design intent of the overall development.

In the historic district, the signage will complement the existing architectural composition of the surrounding neighborhood, drawing from the rich history of landmarks and eclectic mix of longstanding local structures. As development transitions to the south, it is anticipated that the signage will follow the style and motif of the buildings as described in the Architectural Character section of these guidelines. Signage will utilize materials typical of older industrial uses – metal, wood, painted brick to further reference the area’s industrial past in the transitional area. While larger signage is more appropriate on the high-rise buildings on the southern edge of the district to ensure visibility on taller structures.

Given the range of signage likely due to the transitional nature of the development, flexibility in allocation of signage square footage is desirable – as outlined in the PDA zoning code waivers. In all instances however, the signage will reflect the larger vision of the Gas Worx neighborhood



Signage

The use of distinct gateway signage will serve as a marker for entry at key locations and an iconic visual element to convey the project identity. Larger primary gateway signage is envisioned at the intersection of Nuccio Pkwy. and E. 4th Ave., as this will be a primary pedestrian and vehicular entry point into the neighborhood. Smaller secondary signage is envisioned at specific gateway locations from the south, as well as along the Paseo at the commercial heart of the neighborhood

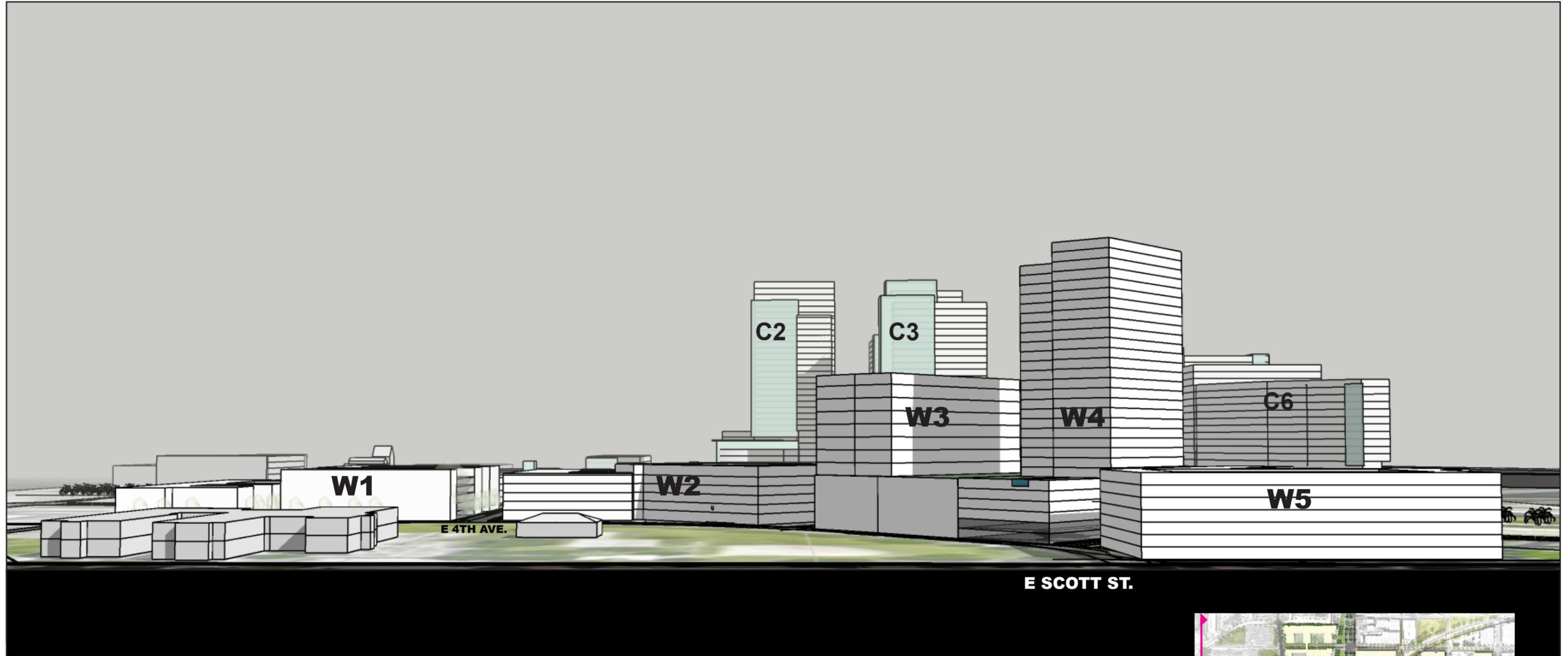
Gateway signage design is to be at an urban scale, large enough for distant legibility, and comprised of verbiage and/or architectural or sculptural artistic elements and illuminated. Signs will be a mix of both freestanding forms and/or attached to buildings. While signage design throughout the neighborhood will have variations, they will be designed with a common aesthetic thread to ensure cohesion.

While specific location, quantity, and design of gateway signage is to be determined, precedent imagery here is suggestive of the scale, type, and vibrancy.



Building Elevations and Sections

West Parcels Section



Building Elevations and Sections

Central Parcels Section



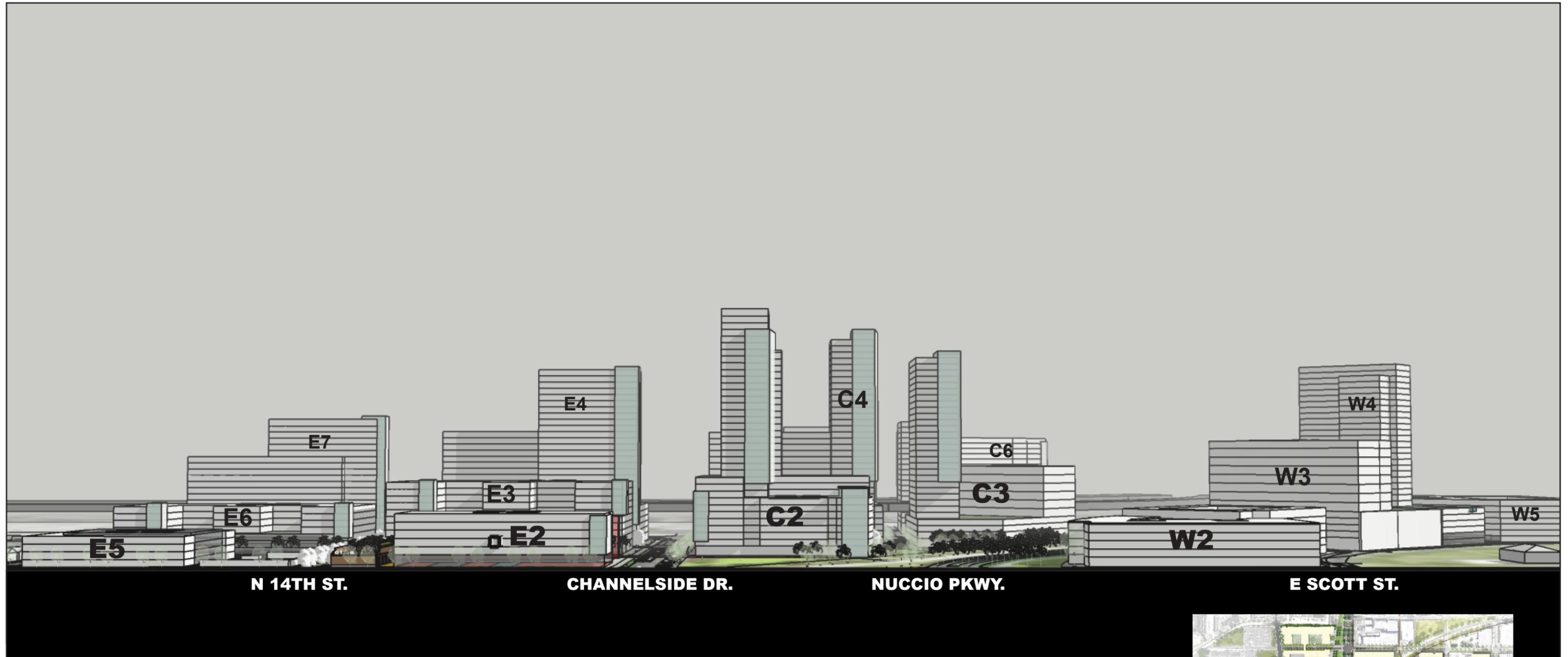
Building Elevations and Sections

East Parcels Section



Building Elevations and Sections

All Parcels Section



Typical Elevations

Introduction

The elevation guidelines establish the major organizing architecture principles in the district. The guidelines seek to create a cohesive language yet allow enough variety to create a varied and rich experience.

Each block should address the street at the base with a pedestrian scaled architecture and detailing. The mid to high-rise scale blocks should ideally have a zone of activation at the street to provide a rich and varied experience for the pedestrian. This can be accomplished by the use of retail, residential or office amenities and lobbies, or residential walk-up units.

The middle portion of each block consists of the most typical facade articulation for the block. The residential blocks will typically have their windows and/or doors organized on a 12' module. The office blocks will typically have their windows organized on a 5' module.

The top zone of each block will ideally have either a cornice element at the roof or a varied architectural expression for one or multiple floors. The cornice element can be composed of added architectural detailing at the roof parapet or a projecting canopy. This top zone could also consist of one or multiple floors with a different architectural treatment. This treatment could consist of a change of color, material, or window articulation.



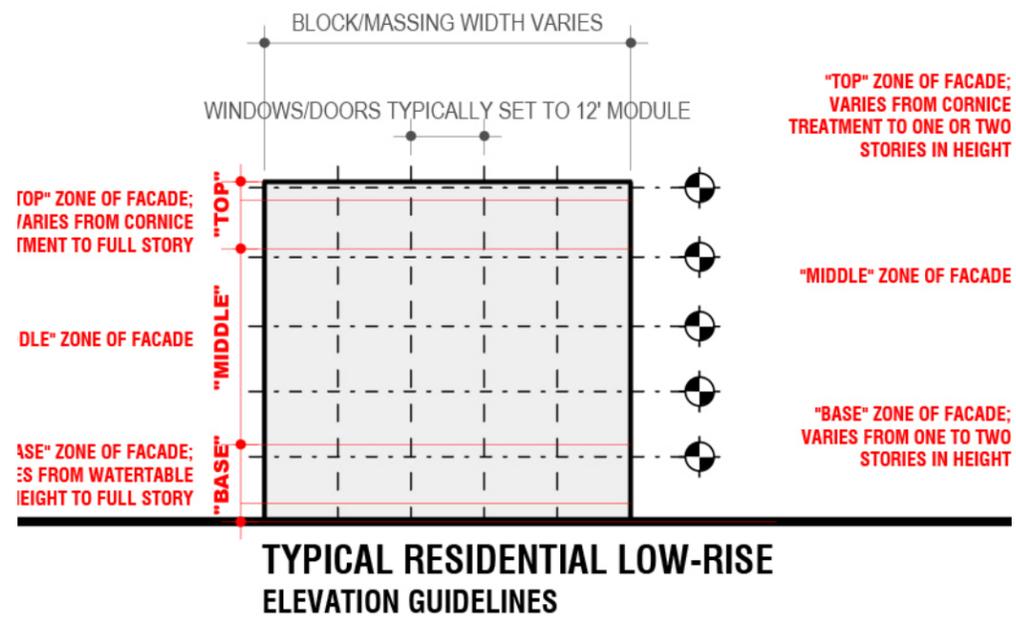


Typical Elevations

Typical Low-Rise Residential Elevation



Low-Rise Buildings Indicated In Masterplan



Building Precedents

Typical Elevations

Typical Mid-Rise Residential Elevation

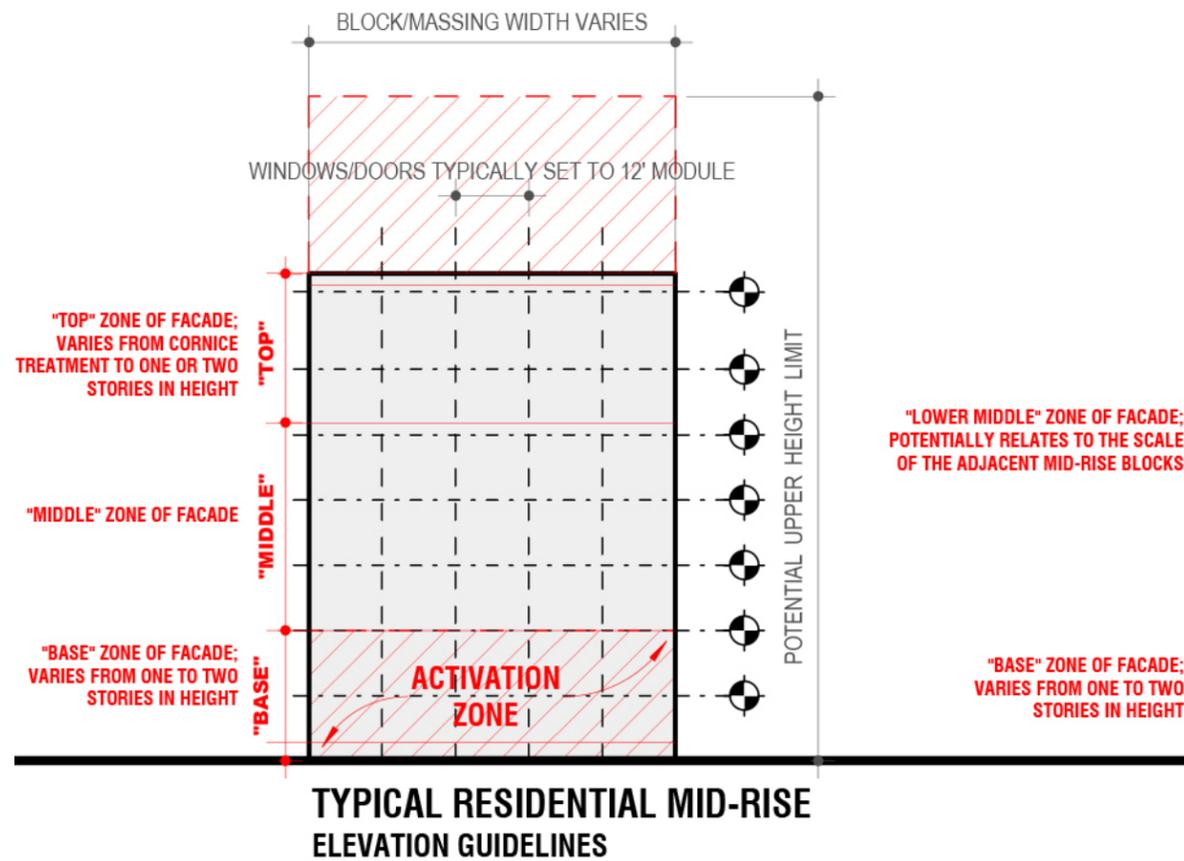


Mid-Rise Buildings Indicated In Masterplan

"UPPER MIDDLE" ZONE OF FACADE;
THE PRIMARY FACADE OF THE FACADE

"LOWER MIDDLE" ZONE OF FACADE;
POTENTIALLY RELATES TO THE SCALE
OF THE ADJACENT MID-RISE BLOCKS

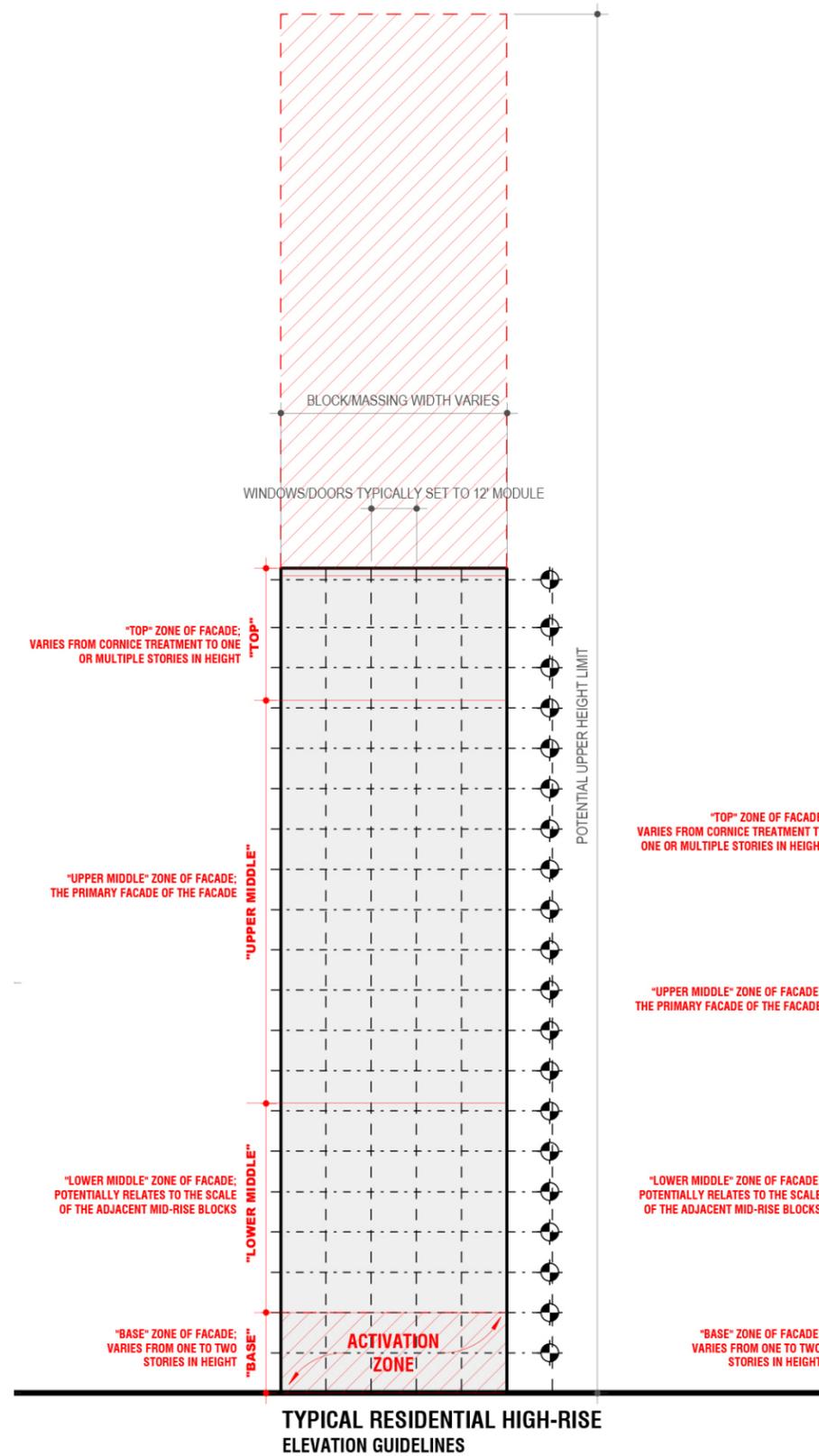
"BASE" ZONE OF FACADE;
VARIES FROM ONE TO TWO
STORIES IN HEIGHT



Building Precedents

Typical Elevations

Typical High-Rise Residential Elevation



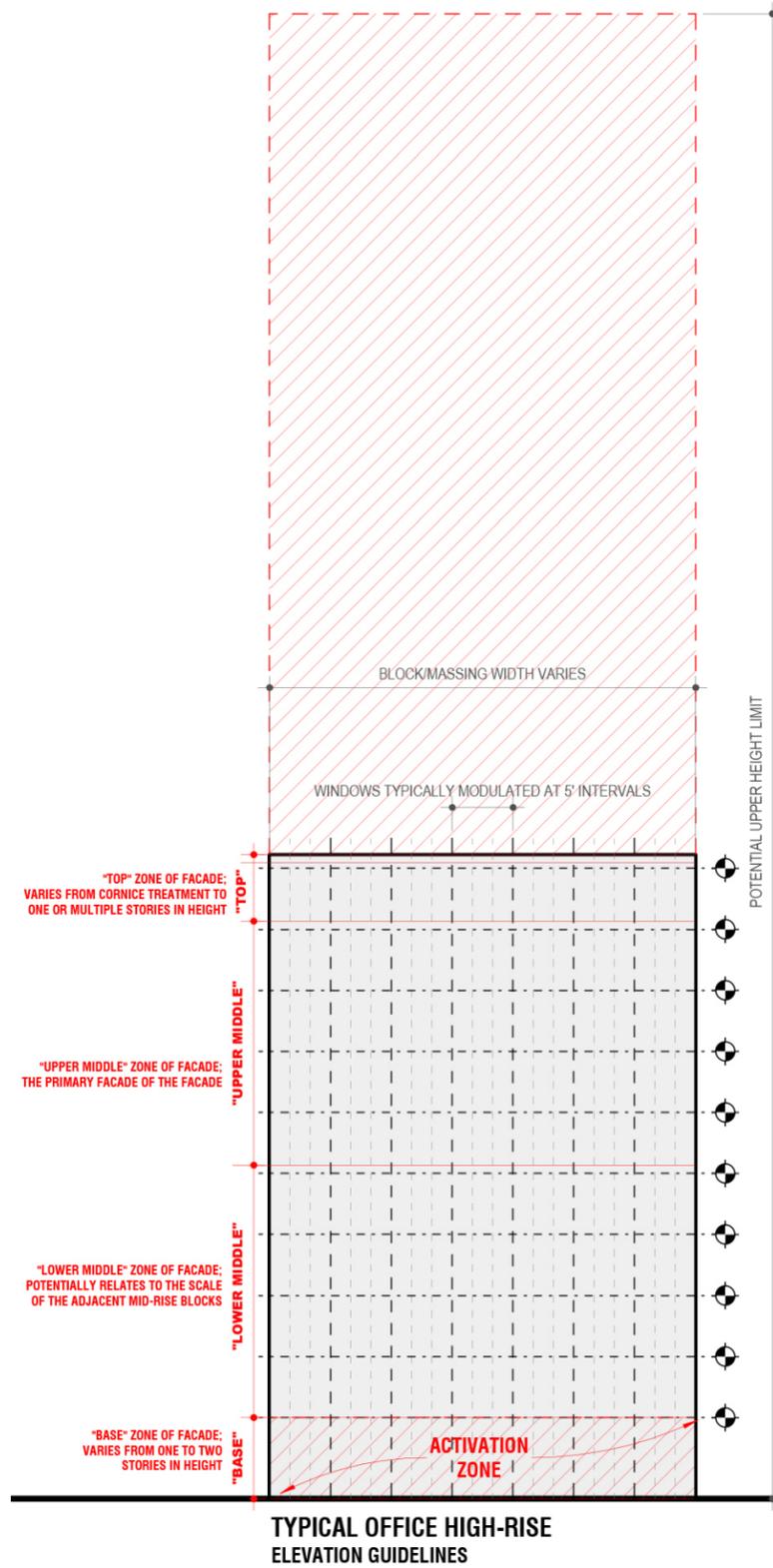
High-Rise Buildings Indicated In Masterplan



Building Precedents

Typical Elevations

Typical High-Rise Office Elevation



High-Rise Buildings Indicated In Masterplan



Building Precedents

Streetscape

General Street Standards

This section describes the guidelines for the streetscape along newly constructed streets with the Gas Worx area. These general guidelines apply to all streets, and in addition, City Street standards shall apply to public streets within the redevelopment. Each specific street is described and illustrated in the sections that follow.

Streets shall be designed to promote pedestrian and bicycle access, circulation, and safety. In addition some streets have parallel parking and/or loading at curbside as well as drop-off areas along the curb. The condition of existing streets in Gas Worx varies. Some streets only require some new sidewalk paving adjacent to new buildings or some additional street tree planting where gaps or missing trees occur along the sidewalk. Other streets, such Second Avenue, require rebuilding including new curbs, street pavement, sidewalks, street tree planting, lighting and street furnishings. Each street will be evaluated in the future design phase to determine the level of improvements that are required.

Streets are designed to provide access to Gas Worx and adjacent areas but also provide access to parking garages and service areas. Parking garage entries and service entries should be located, where possible, on streets with lower pedestrian volumes. Vehicular traffic will also access curbside parking spaces as well as laybys to drop off and pick up passengers.

Bicyclists will share the streets with motorists because the traffic is relatively slow moving. With the sharing of the street ROW, these streets will be considered “bicycle boulevards.” The street will have markings to identify that bicyclists will be using the corridors as well as motorists. Where possible in coordination with the City of Tampa, efforts will be made to install barriers to completely separate vehicular traffic from bicyclists and pedestrians.

Intersections between streets as well as intersections of private access roads and streets shall be carefully designed to consider the variable needs of pedestrians, bicycles, automobile traffic, bus traffic, delivery trucks and emergency service vehicles. Where possible, some layby or drop-off areas are planned to allow for safe access to ride sharing vehicles.

Some streets may incorporate the historic pattern of paving materials in Ybor City with surfaces of brick or unit pavers in selected locations, for example in high volume pedestrian areas or at key intersections. Key intersections may also be designed as raised or tabled intersections to slow traffic and express the significance of pedestrian crossings. In addition, the blocks which are longer in the east west direction may also have mid-block crosswalks which may be raised to the sidewalk levels to slow traffic for safe pedestrian crossings.

Lighting shall balance concerns between light pollution, safety, and the creation of a compelling evening streetscape. Some existing street lighting will remain, while some streets will require new or supplemental lighting. Lighting should vary by area within the Gas Worx depending on the predominant patterns of use. Contemporary lighting that is influenced by the design of historic fixtures of Ybor City will be considered for the lighting in the northern blocks within or adjacent to the historic district. In the southern blocks where higher density buildings are located, the lighting may have a more distinctive contemporary design. Bike racks shall be included in a manner consistent with zoning for short or long term bike users.

In general, sidewalks will be shaded by existing trees and/or new street tree planting. Some existing streets with shade trees will be supplemented by adding new street trees. Some of the streets have overhead electrical transmission and distribution lines. In many cases these will remain, as these elements are consistent with the historical character of Ybor City and the industrial character of Gas Worx near the waterfront. For example, historic Seventh Avenue to the north is characterized by poles and power lines as part of the streetscape. Street tree planting along sidewalks will be located to avoid overhead lines, or in species approved by TECO for planting under power lines.

Planted areas in the sidewalk zone shall be designed to allow for water infiltration in areas with less intensive pedestrian traffic. In high volume pedestrian areas sidewalks will be designed to maximize pedestrian use. Street furnishings will be selected to complement the historic materials and patterns in Ybor City. This will include the use of decorative metals as well as colors prevalent in the historic streetscapes of Ybor City. The use of materials and the design expression of the street furnishings will also be influenced by the industrial character of the Gas Worx context with a simple functional design expression using durable materials.

General Note:

(1) The minimum sidewalk area / dimension includes area occupied by street furnishings, street trees / plantings, mini-kiosks and other elements to enrich or enhance the pedestrian’s comfort and experience, subject to design approval by the City.

(2) Encroachment within the minimum sidewalk area is permitted provided:

a. Awnings, canopies associated with doorways, windows or other building façade openings shall be at least 9’ above sidewalk grade.

b. Cantilevered elements, balconies or other similar building extensions shall be at least 15’ above sidewalk grade.

Streetscape

Sidewalks and Planting

This section describes the guidelines for the streetscape and publicly accessible pathways in Gas Worx. Sidewalks should have a minimum clear unobstructed walking width of five feet. This is a minimum width; typically adjacent to street tree planting areas. Paving surfaces for use by pedestrians will widen between trees and at intersections. In the parcels in the northern part of Gas Worx east of Nuccio Parkway, the sidewalk will be wider to allow for more intense pedestrian traffic and for tenants to locate outdoor dining or other features along storefronts. A clear unencumbered walking zone of at least five feet will be maintained.

Street trees will be located in appropriately scaled planting areas throughout the district to support the growth of canopy shade trees. Palms may be planted on selected streets. Streets will have trees selected from the City of Tampa list of approved species. Street tree planting should be designed to provide shaded walks, particularly along south and west facing buildings. Where retail is planned along these corridors, street trees should be selected among those approved by the City of Tampa and which have a habit of growing upright with a significant clear space under the canopy to avoid blocking views to retail storefronts.

Planting areas for street trees at curbside could also accommodate perennials, low shrubs and native grasses. In the most intensely used pedestrian areas, these planting areas may benefit from low railings to protect plantings and prevent compaction of soil zones. The planting zone along the curb should be a minimum width of 3 feet. Where curbside parking occurs, there should be at least 12 inches from the face of curb to the planting zone, although on some selected streets this may be reduced to 6 inches.

Street trees will be located along sidewalks and pedestrian ways, except where the planting of trees is restricted by overhead power lines or other easements. In locations with overhead power lines, tree species will be limited to those approved by TECO for planting under power lines. Some alleys, or minor local residential streets, which are not intended for pedestrian circulation may not have street trees. Palm trees will be planted in selected locations, for example along Nuccio Parkway, where a larger scale civic expression with multiple rows of palm trees is appropriate for this significant corridor.

Street trees should be selected and sized at a minimum 2.5” caliper, and will be planted at curbside, at least 2 feet from the face of curb. The spacing should average no more than 30 feet on center along the curbside, however in selected locations such as at building entries with canopies and balconies, there may be areas where trees are spaced further than 30 feet apart.

Street trees should be planted with an adequate soil zone along with levels of oxygen, soil nutrients and water to sustain healthy growth. The strategies for creating these planting zones to support healthy trees should vary depending on the intensity of pedestrian traffic, therefore high volume pedestrian areas should have a different approach to street tree planting than in residential areas where there are larger planting areas. Some areas in the high volume pedestrian zone may have intense pedestrian activity where the curbside planting should be reduced in favor of pavers over the amended soil zone. In addition to the curbside planting, there should be movable planters in some areas to define outdoor dining areas, seating areas, direct pedestrian circulation as well as soften paved areas.

The existing east-west streets in Ybor City and in Gas Worx typically have a 60’ ROW. The design character of these streets will vary slightly depending on the adjacent uses and the intensity of the development. Residential blocks may have less paving in the sidewalk zone and broader planting areas along the sidewalk. Sidewalks in residential areas may have amenity spaces that open up to the sidewalk for outdoor use, as well as unit entries, stoops, and patios along the sidewalk intermixed with planting. There may be numerous entries to buildings along these sidewalks where awnings and canopies may extend over the sidewalk.

Some selected streets particularly in the north part of the district where lower scale residential buildings are located will be a ROW of 50’. These residential blocks along the 50’ ROW will have narrow sidewalks with narrow planting zones along the curb with palm trees the preferred street tree due to the tighter urban conditions. There may be entries to ground floor units along the sidewalk in these buildings. Sidewalks along residential buildings may have amenity spaces that open up to the sidewalk for outdoor use, as well as unit entries, stoops, and patios along the sidewalk intermixed with planting.

Paving of pedestrian areas should be cast-in-place concrete, exposed aggregate paving, and/or pavers in key locations. Where cast-in-place concrete is the material, there should be scoring lines spaced to give scale and rhythm to the pedestrian zone as compared to the larger scale spacing of scoring lines in vehicular surfaces.

Street Design and Typology

Gas Worx Street Plan

New Streets

Constructed with adjacent developments. Private streets built to public standards that can be dedicated to the City in the future. Specific street sections provided below.

Reconstructed Streets

Existing streets that will include a combination of travel lane realignment, addition of street parking, stormwater reconstruction, new sidewalks, and street trees. Details to be provided by block with adjacent development site plans.

Other Existing Streets

All other existing streets to remain with typical adjustments for in-fill development, e.g., new curb cuts, replacement curbing, sidewalks, accessibility ramps. Where space allows, new streets trees, benches, bike racks and other enhancements will be added.

Major Improvements

A. Nuccio Parkway: Realignment and consolidation of existing travel lanes is proposed with new intersections for east-west streets. This project will be in coordination with the City of Tampa and potentially Brightline and/or other rail service.

B. Third Avenue Paseo: Pedestrian only thoroughfare with outdoor dining area.

C. New CSX Crossing: Vehicular crossing connecting Adamo Drive and Scott Drive west of Nuccio. Crossings will be subject to CSX, FDOT approval and in coordination with the City of Tampa.

D. Roundabout Replacement: Removal of existing roundabout at the Channelside Drive and 4th Avenue intersection to be replaced with conventional T-intersection and realigned CSX rail crossing. This project is on City property and will be part of the new adjacent park in coordination with the City of Tampa and CSX.



- New Streets**
- Reconstructed Streets**
- A **Nuccio Parkway Realignment**
- B **3rd Avenue Paseo**
- C **New CSX Crossing**
- D **Roundabout Replacement**

Street Design and Typology

New Streets

East Scott Drive Extension /New CSX Crossing

This road will provide a key east-west connection linking Adamo Drive to Nuccio Parkway, subject to the approval of a new railroad crossing by CSX. Provided the crossing is approved, it is anticipated that the new road will have a full movement intersection with the realigned Nuccio Parkway (see below).

a. Nuccio Parkway to 12th Street

This portion will have two travel lanes with street parking on one side and sidewalks and trees.

b. 12th Street to Channelside Drive

At 12th Street, eastbound traffic will be required to turn south on 12th with the road converting to a one-way (westbound) from the Channelside to 12th. This will allow westbound traffic on Adamo to connect to Nuccio Parkway.

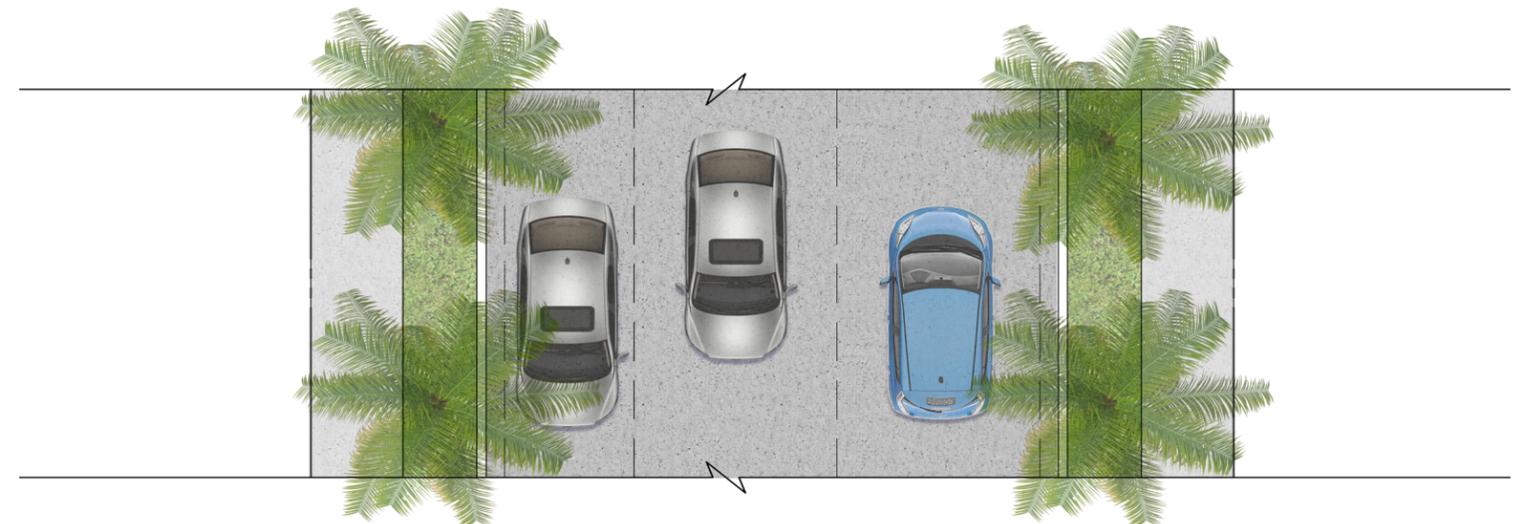
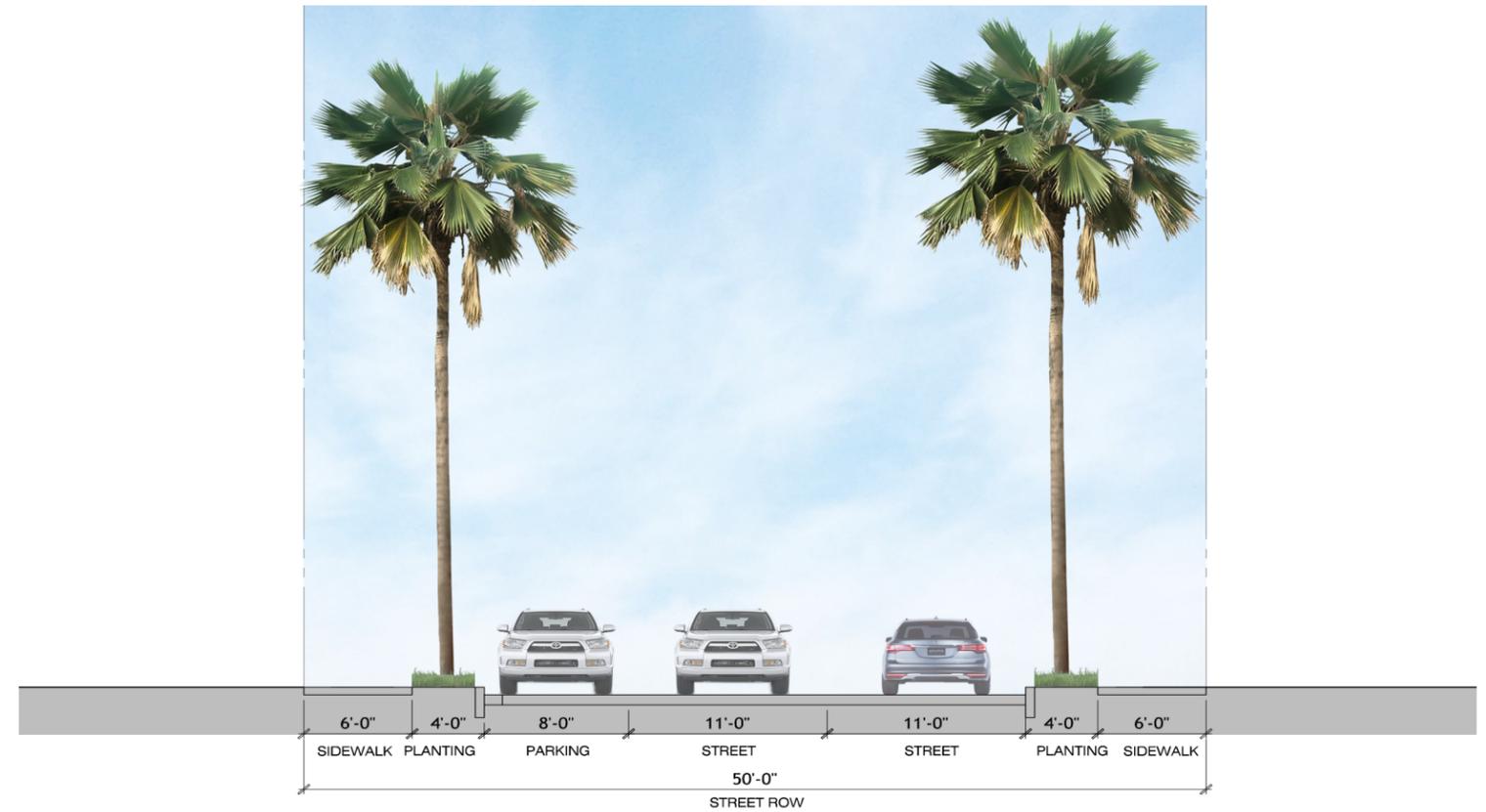
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(2) Encroachment within the minimum sidewalk area is permitted provided:

a. Awnings, canopies associated with doorways, windows or other building façade openings shall be at least 9' above sidewalk grade.

b. Cantilevered elements, balconies or other similar building extensions shall be at least 15' above sidewalk grade.



Street Design and Typology

New Streets

12th Street Extension

Pearl Street will be a new road extending the existing 12th Street to 2nd Avenue. The street will have two travel lanes with street parking on one side and sidewalks and trees.

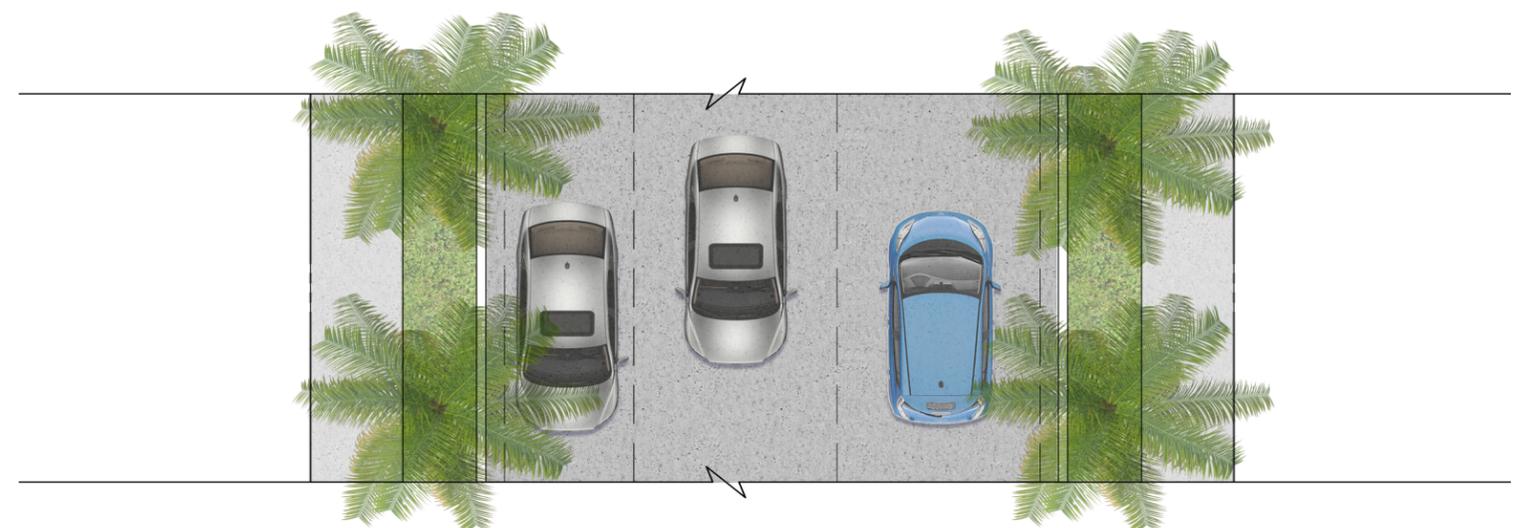
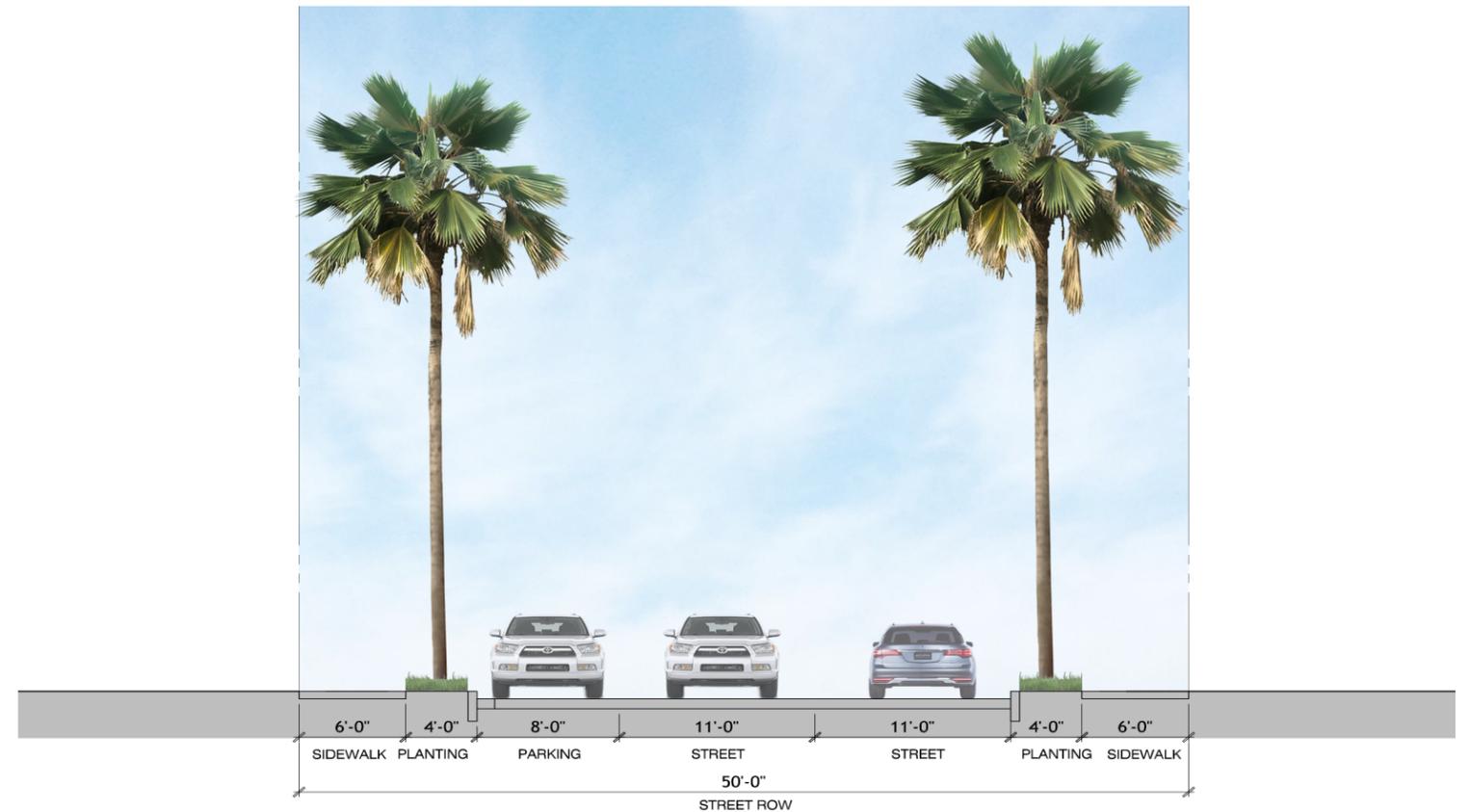
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Street Design and Typology

New Streets

2nd Ave Extension

This new section of road will continue 2nd Avenue west of Channelside Drive and maintain the proposed design of 2nd Avenue as a significant residential street with Gas Worx. As such, the street will have two travel lanes with street parking on both sides and sidewalks and trees on both sides.

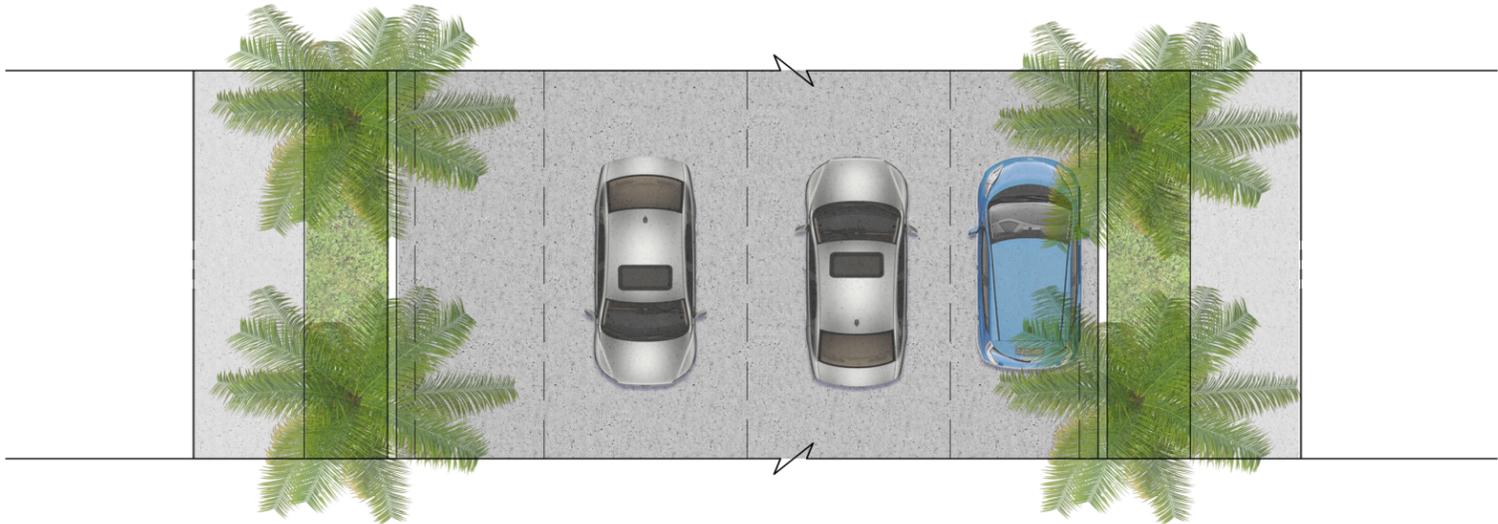
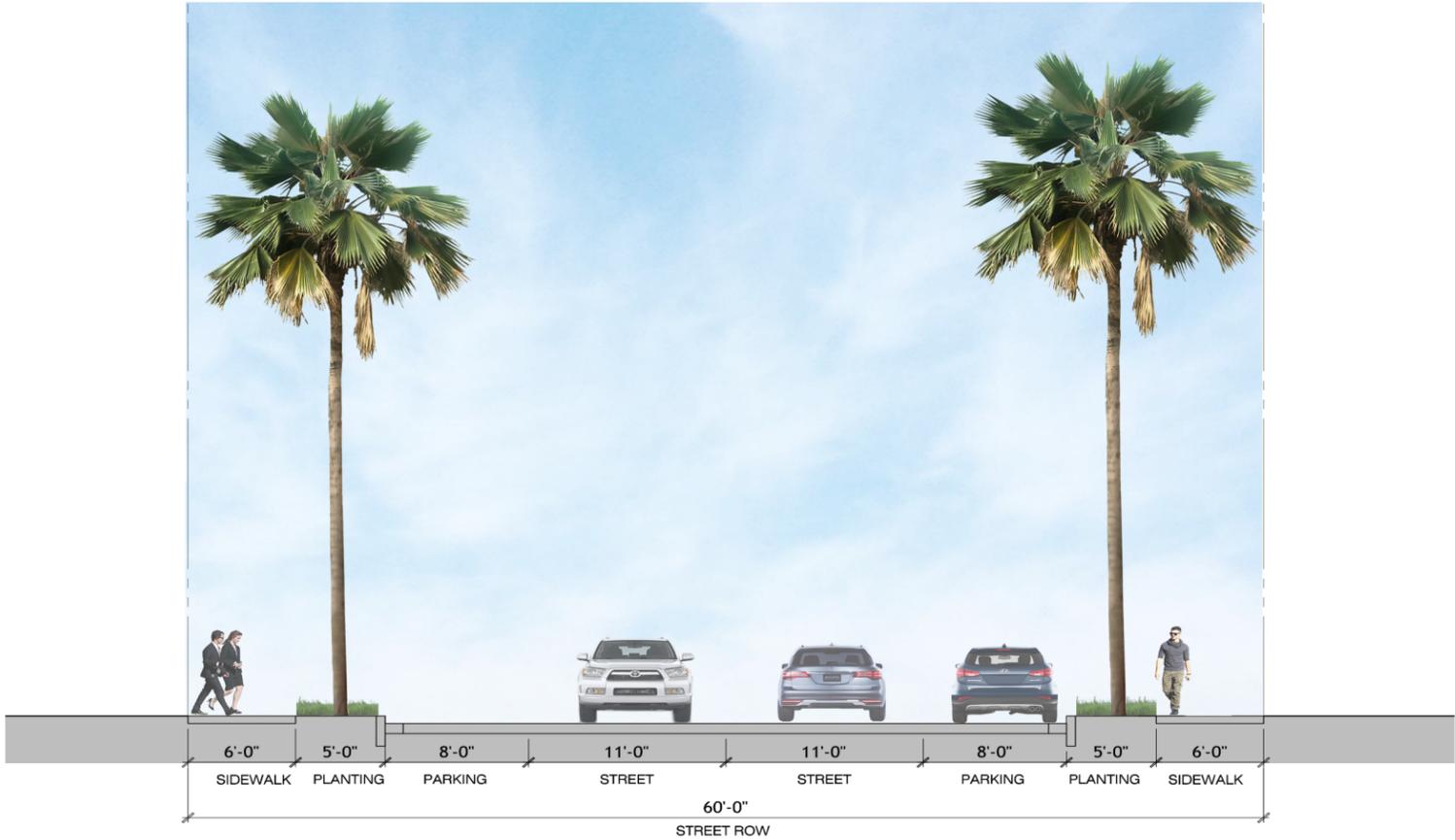
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Street Design and Typology

Reconstructed Street Sections

2nd Avenue (Channelside to 15th Street)

The current 2nd Avenue right of way consists of a wide paved street, poor quality curbing and no sidewalks. The north side of the street is also encumbered by above and below grade utilities. Given that 2nd Avenue will be a main thoroughfare and serve the highest density within Gas Worx, the plan calls to rebuild these blocks with two travel lanes, street parking on both sides and sidewalks and trees on both sides.

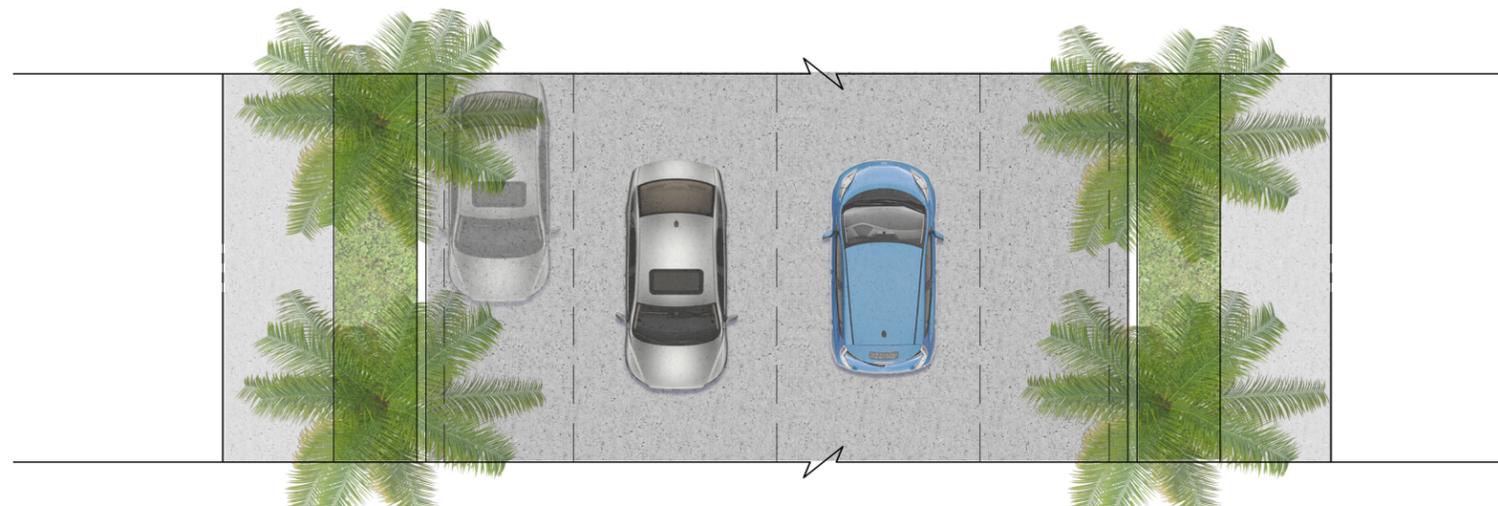
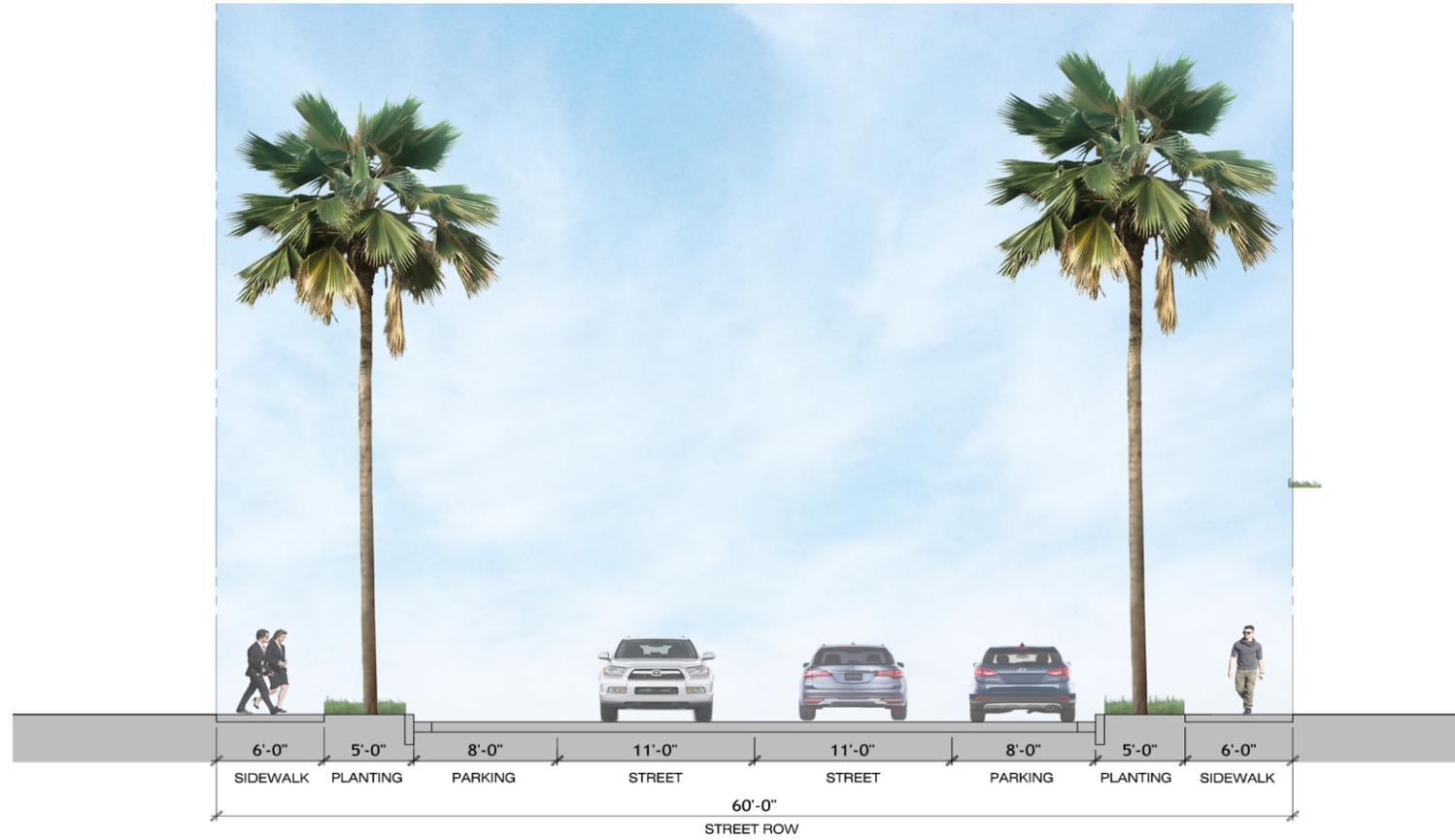
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Street Design and Typology

Reconstructed Street Sections

14th Street (Adamo to 2nd Ave)

The existing condition of this block of 14th Street consists of irregular paving with no curb or sidewalk. It also does not connect to Adamo Drive but serves as a dead-end drive. This section of road will be rebuilt with two travel lanes, though proposed as right-in/right-out only on Adamo. Sidewalks and trees will be added on both sides; however, it is anticipated that service and loading for buildings on 2nd Avenue will be located on this side street.

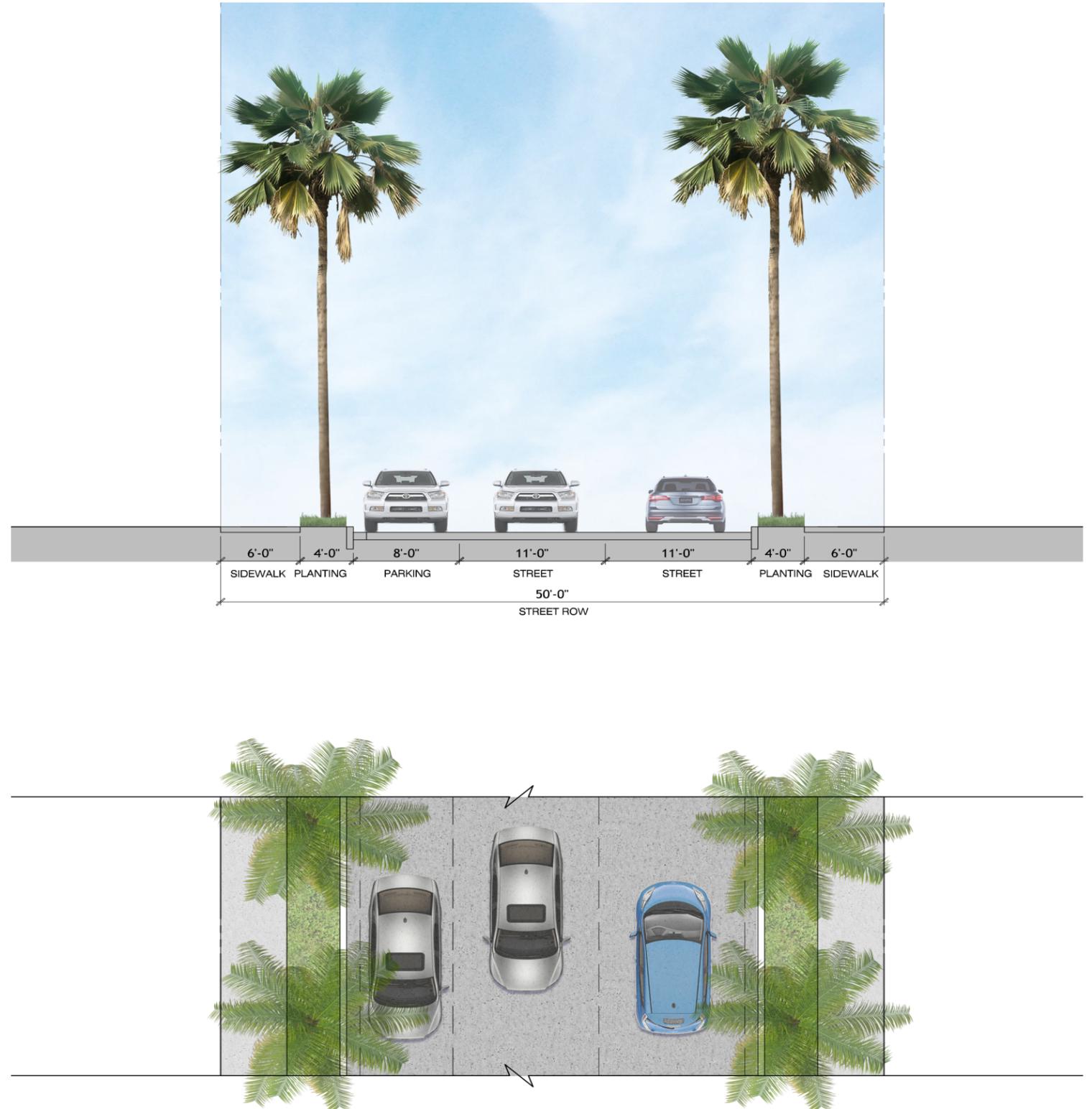
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Street Design and Typology

Reconstructed Street Sections

3rd Avenue (14th to 15th Streets)

The existing condition of this block of 3rd Avenue consists of irregular paving with no curb or sidewalk. The right of way also includes TECO transmission lines and poles. This section of road will be rebuilt with two travel lanes and sidewalks connecting 15th Street to the pedestrianized section of 3rd Avenue on the next block west.

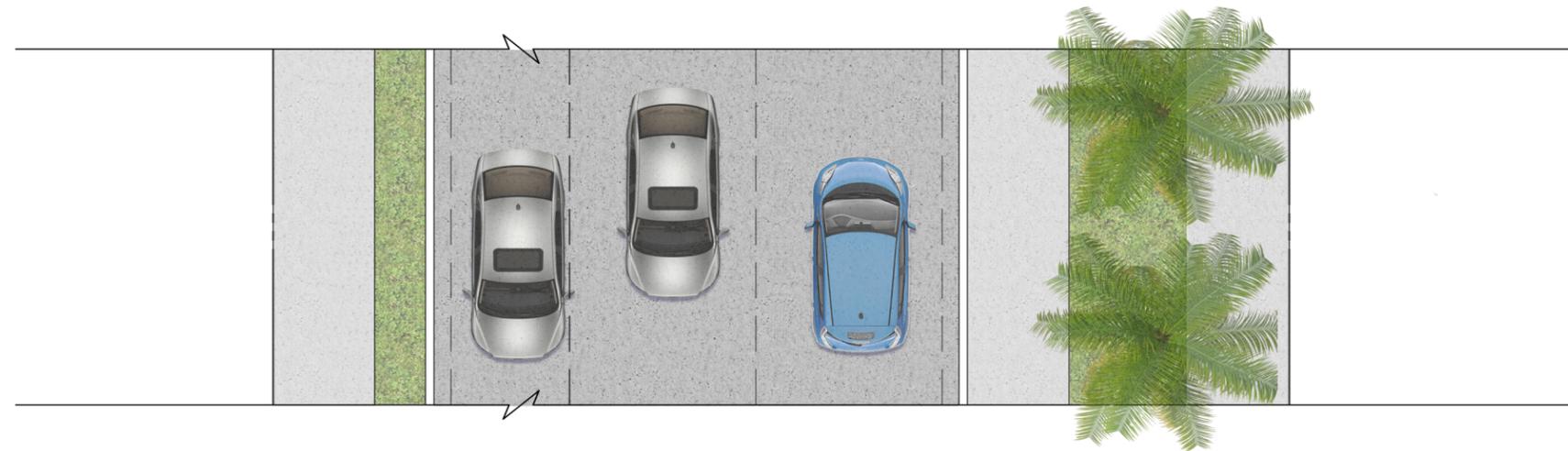
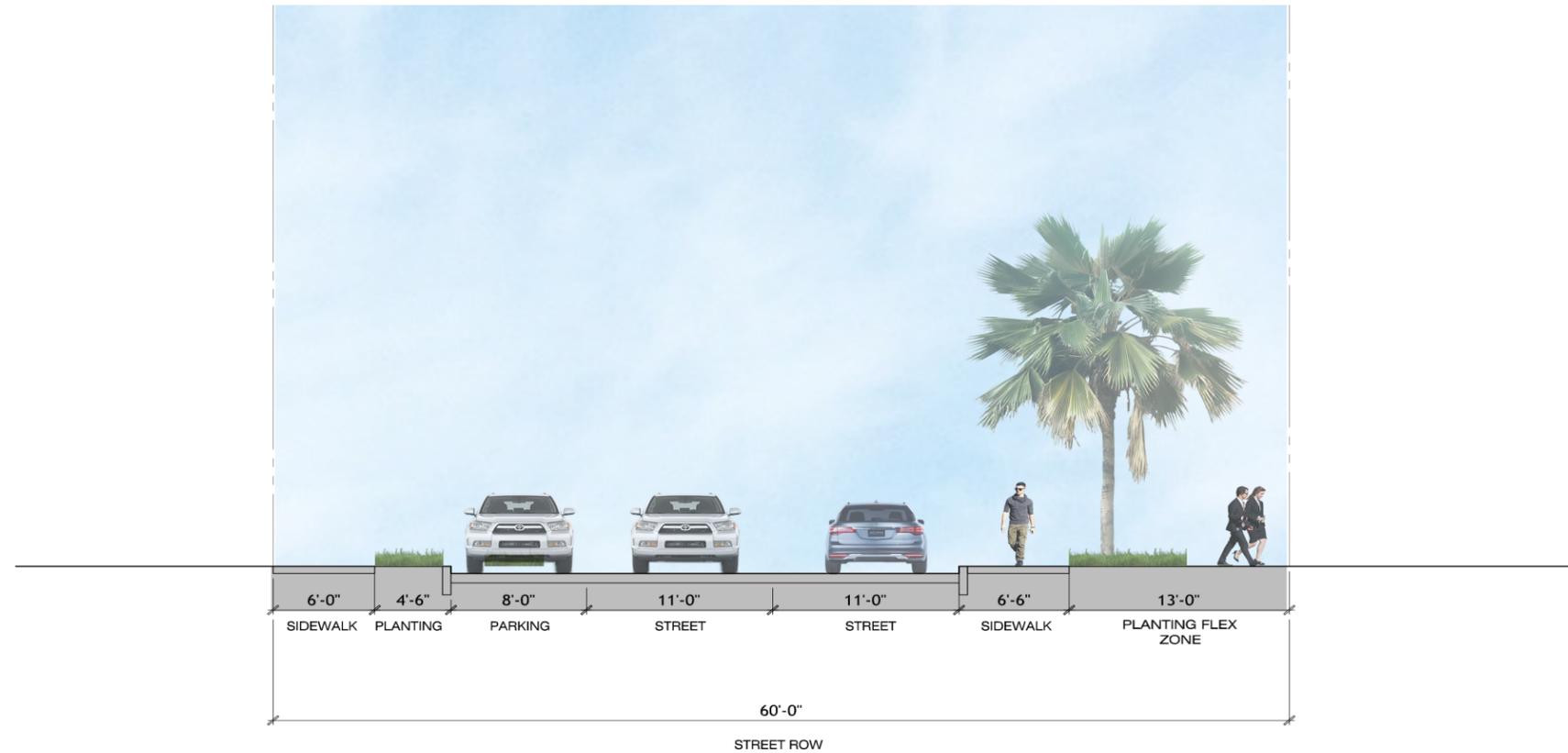
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Public-Private Infrastructure Projects

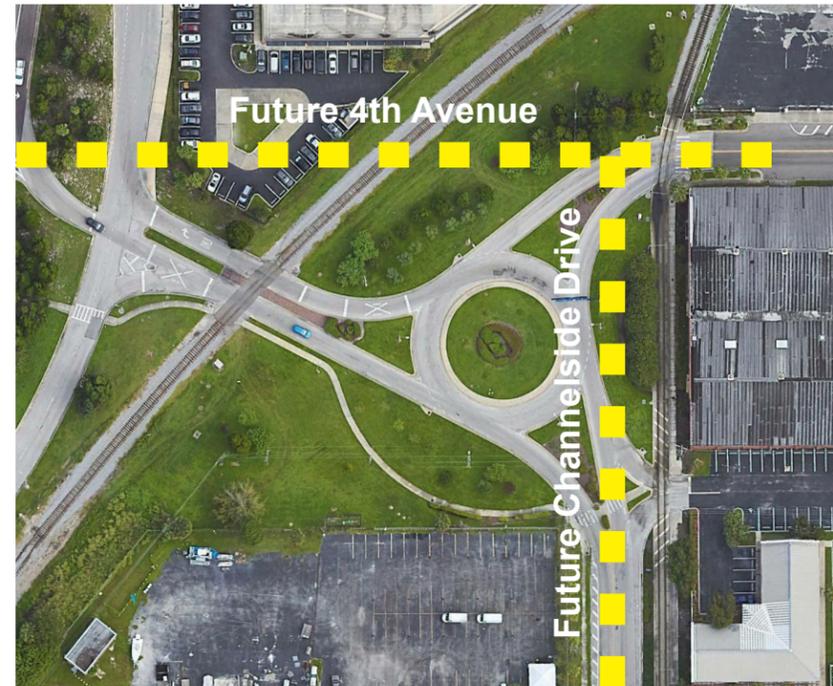
Channelside Drive Roundabout/4th Avenue CSX Crossing

Channelside Drive is a significant connection to and from downtown and the Channel District to Ybor City. Existing Channelside Drive terminates in the Gas Worx district at a roundabout that connects to Fourth Avenue to the east and to Nuccio Parkway to the west.

It is proposed to remove the roundabout and terminate Channelside Drive in a new T intersection at Fourth Avenue to re-establish the earlier and historic framework of a grid of streets. This will improve the pedestrian experience with new sidewalks along Channelside Drive and new crosswalks at intersections to help pedestrians cross Channelside Drive, especially as compared to pedestrian crossings at the existing roundabout. In addition, land reclaimed from the roundabout will be converted to a park at the southwest corner of the new intersection at Fourth Avenue, which is a prominent and highly visible location at the heart of Gas Worx.

A new streetcar stop is proposed along the east side of Channelside Drive between Fourth and Fifth Avenues to serve the Gas Worx district. It is adjacent to a proposed small cluster of food and beverage uses along with a potential market hall.

The design of Channelside Drive includes continuous planting zones curbside however the streetcar line restricts street tree planting along the east side of the street. While street tree planting will be located on the west side of the street, low planting areas at curbside on the east side can accommodate perennials, low shrubs and native grasses depending on the width of the ROW.



Existing Conditions



Proposed Conditions



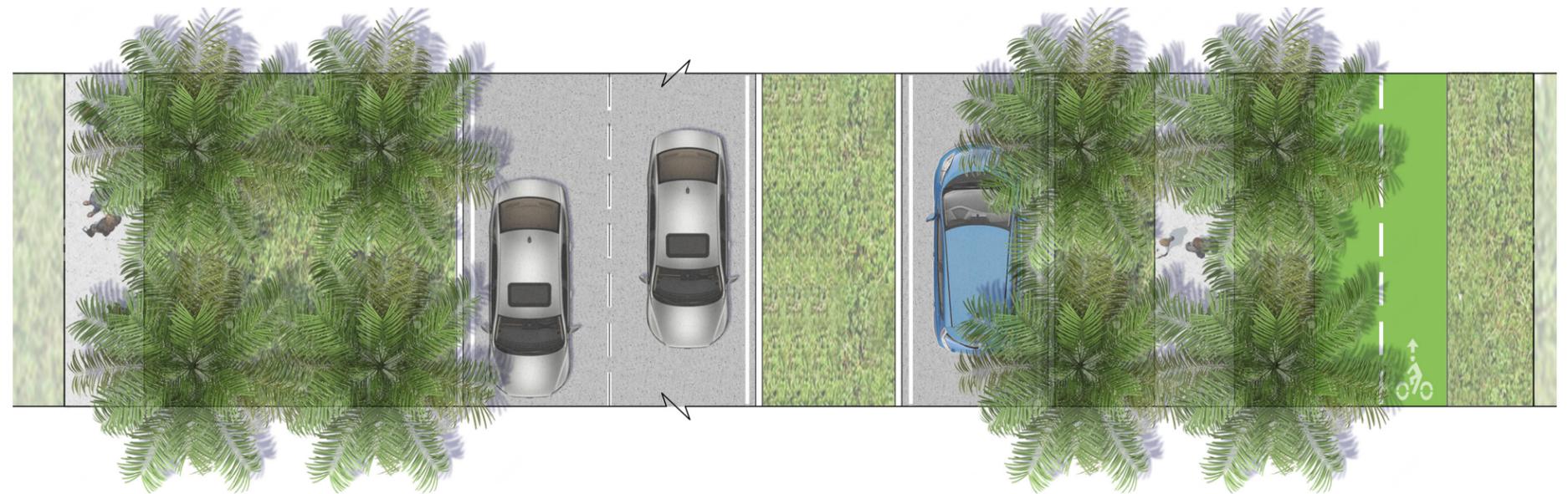
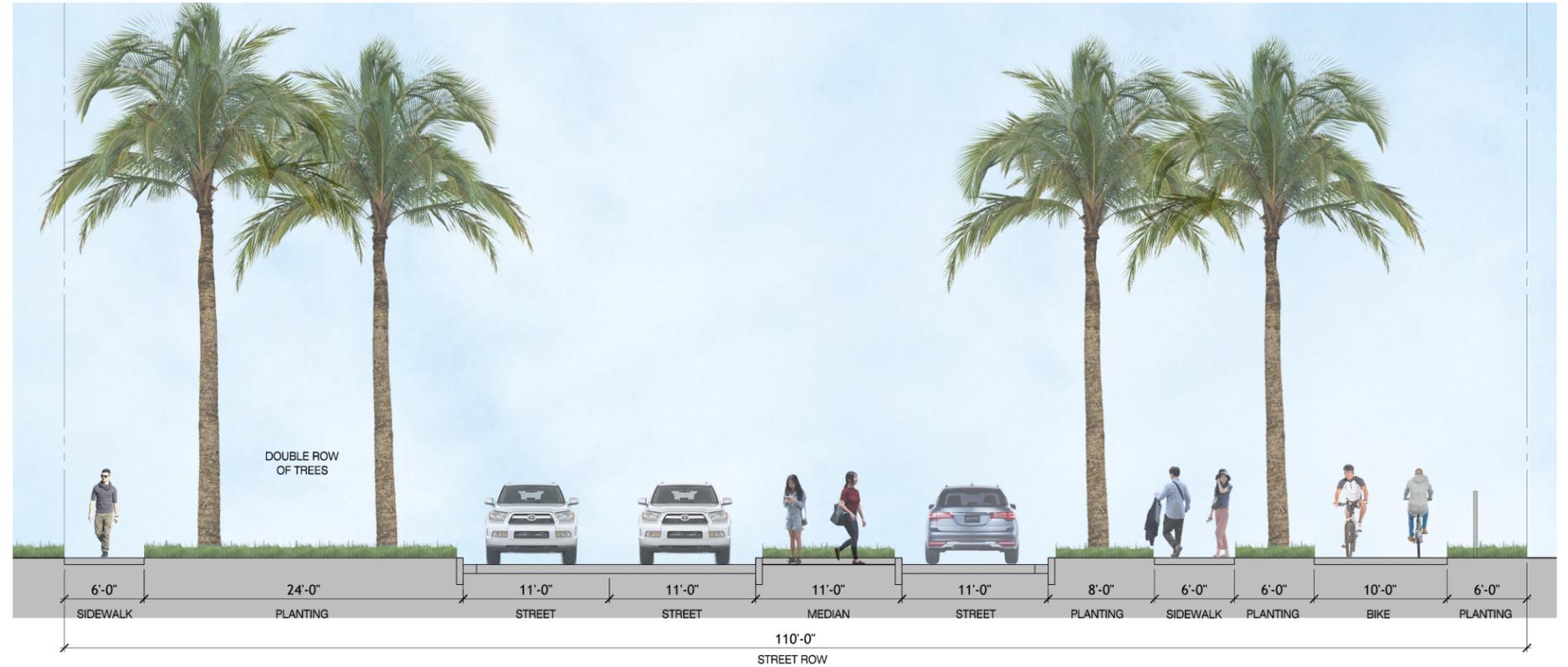
Public-Private Infrastructure Projects

Nuccio Parkway Realignment

Similar to the Channelside roundabout, Nuccio Parkway was designed and constructed in a different era with a focus on circulating vehicular traffic from I4 to downtown Tampa. As such, it also disregards the historic street grid, underutilizes land area and provides limited pedestrian and bike accessibility. Moreover, with the CSX rail tracks, it provides a barrier. The City is currently addressing this through some reconfiguration of travel lanes and the addition of a separated bike lane. The Gas Worx plan calls to further condense the Nuccio Parkway street section adding a higher degree of pedestrian accessibility, particularly east-west crossings.

The Nuccio Parkway right of way (and adjoining City and private land) is also being assessed for new rail service – inter-city rail (i.e., Brightline) and/or light rail. The ultimate configuration of a redesigned Nuccio Parkway will be established through discussions with the City and rail service providers. Moreover, it is expected the re-construction of Nuccio will be part of a larger project outside of the Gas Worx applicant’s scope of work.

Within that context, the section shown is one possible design, highlighting important concepts.



Conceptual Section - Subject to incorporation of rail infrastructure

Public-Private Infrastructure Projects

New Streetcar Station

The TECO Line Streetcar (operated by HART) connecting downtown and Ybor City runs along the east side of Channelside Drive through the center of Gas Worx. Currently the nearest stations are the Publix Station at Madison Drive in the Channel District and the Cadrecha Plaza Station on 13th Street, between 7th and 8th Avenue. As such, there is .6 mile gap between stations. With the density of Gas Worx a new station is warranted to fill this gap. Moreover, with a commercial hub and park space proposed between 3rd and 4th Avenues a station in this location will generate ridership, while providing a convenient transit alternative to residents and commuters heading to Channel District, Water Street, Downtown and Tampa Heights (once the service is extended).

Construction of the new station will be coordinated with HART and the City of Tampa to ensure it is safely located east of Channelside Drive and provides necessary accessibility.



Station Location



Community Gathering Areas/Public Spaces Gas Worx Park

Ybor City has developed with a rich and colorful history but has few parks and green spaces. Within Gas Worx, it is proposed to create a new centrally located park that will serve as a gathering space in this part of Ybor City. It is important to make the park relatively small and compact to respect the scale and walkability of historic Ybor City. Spaces that are too large may feel inappropriate and unwelcome in this context. Historically, cities in hot climates successfully made small shaded spaces and courtyards as a natural response to the climate.

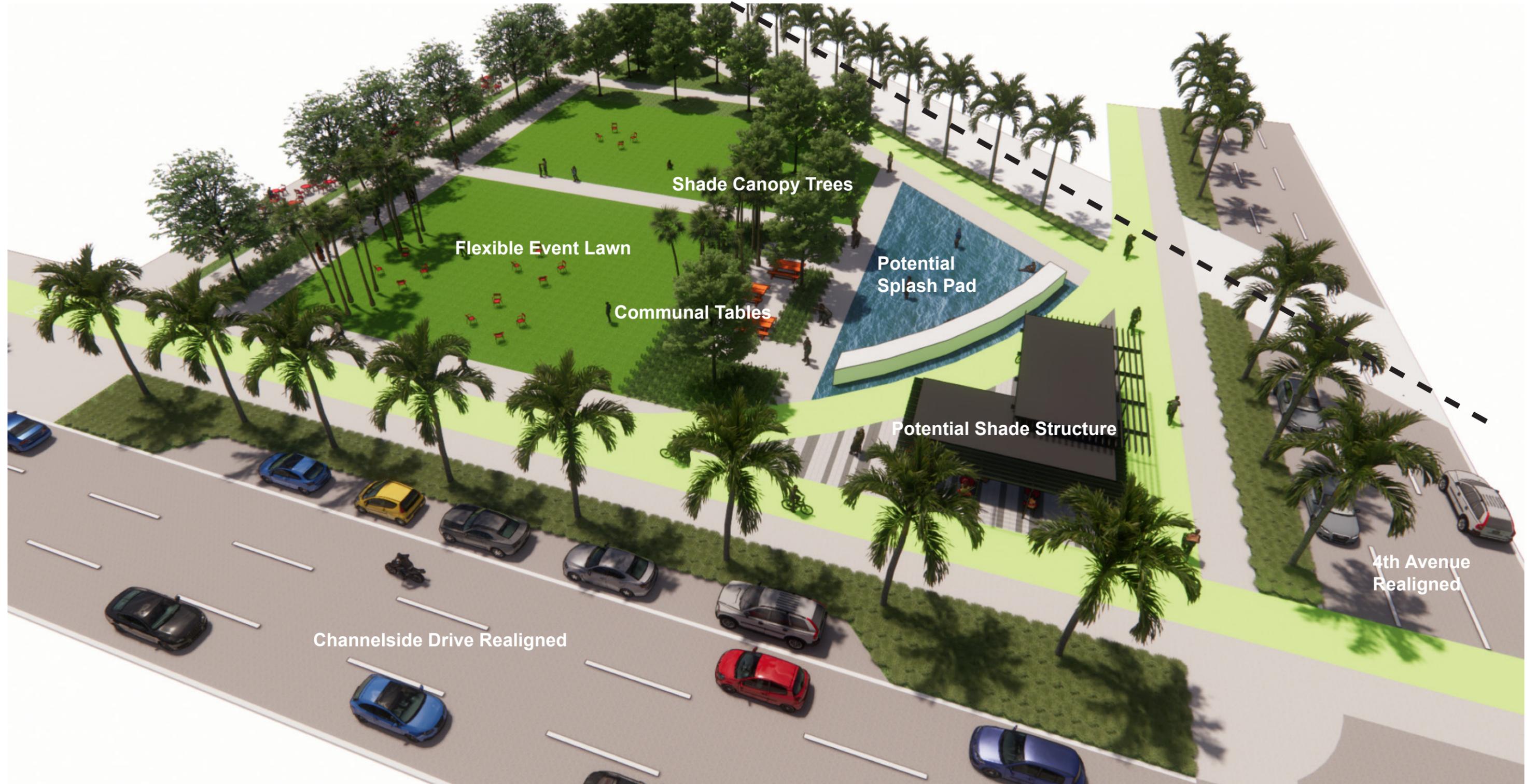
A new 1.5 acre park is proposed along Chanelside Drive west of the new TECO Streetcar line stop, at the center of the district. Some of the land for this park comes from removing the roundabout and making a more conventional intersection. This much needed green space in Ybor City will provide some recreational use as well as opportunities for programming civic and community events. The design of this park at the southwest corner of the new intersection at Fourth Avenue, should have landmark features at this key location with a possible sculptural shade structure, abundant planting and potential water feature.

The programming in the park will be varied to appeal to all ages and range from passive use to some active recreation. Tree planting and shade structures are essential to make the space usable in this climate. Umbrellas, parasols or other potential shading devices may be considered to enhance comfort in the park. Water can have a cooling effect even if only psychological in a hot humid climate.

The 3rd Avenue Paseo will connect directly into the southern edge of the park providing a connection between the open space and the commercial activity focused in and around the warehouse building. It will further provide a opportunity for retail to extend along the base of the buildings fronting the park.



Community Gathering Areas/Public Spaces
Gas Worx Park



Community Gathering Areas/Public Spaces 3rd Avenue Paseo and Plaza

The vacated 3rd Avenue right-of-way between Channelside Drive and 14th Street/Avenida Republica de Cuba provides an opportunity to create a pedestrian promenade and activated outdoor space that will complement retail activity (in the renovated warehouse) and serve, in part, as an extension of the new park west of Channelside. The 3rd Avenue Paseo will be designed, programmed, and managed to be complementary to the heart of the mixed-use Gas Worx neighborhood. With adjacency to the new streetcar station on Channelside, it will also be a convenient social destination for people from other parts of the city center.

The 3rd Avenue Paseo will include a broad, central pedestrian pathway with paralleling outdoor dining area, adjacent to the warehouse on the north side, and landscaped gardens with space for food trucks and vendors to set up along the south side of the walkway. The primary pedestrian thoroughfare will be 15' wide (to permit emergency vehicle access) and use brick pavers or similar material to recall the brick-paved streets of historic Ybor. A distinct type of paving will be used to designate the area for food trucks and vendor stations.

Outdoor dining will be defined through a planted strip with palm trees. While the gardens will incorporate shade structures, plantings, pathways, seating areas and spaces for outdoor entertainment. The use of lighting will be incorporated to further enhance the festive nature of the Paseo. Site furnishings, including benches, bike racks, and trash receptacles will be provided to increase the utility of the Paseo and will be selected to complement the historic materials and patterns in Ybor City.

Subject to assessment by structural engineers, a further component of the Paseo could include a plaza area enclosed by the existing walls of the warehouse. By retaining the walls, such a space could be used as a more intimate courtyard along the Paseo either for additional outdoor dining or event space for outdoor performances. As noted elsewhere in these guidelines, the Paseo and plaza areas should apply materials and architectural elements that are quintessentially Ybor. This could include the use of decorative metalwork, colorful tiles, and unique light fixtures.



Community Gathering Areas/Public Spaces

3rd Avenue Paseo and Plaza



Illustrative images are potential design concepts

